



Notice

City Commission Regular Meeting

7:00 pm

Monday, August 4, 2014

Commission Chambers, Governmental Center

400 Boardman Avenue

Traverse City, Michigan 49684

Posted and Published 07-31-2014

Meeting informational packet is available for public inspection at the Traverse Area District Library, City Police Station, City Manager's Office and City Clerk's Office.

The City of Traverse City does not discriminate on the basis of disability in the admission or access to, or treatment or employment in, its programs or activities. Penny Hill, Assistant City Manager, 400 Boardman Avenue, Traverse City, Michigan, 49684, 922-4440, TDD: 922-4412, has been designated to coordinate compliance with the non-discrimination requirements contained in Section 35.107 of the Department of Justice regulations. Information concerning the provisions of the Americans with Disabilities Act, and the rights provided thereunder, are available from the ADA Coordinator. If you are planning to attend and you have a disability requiring any special assistance at the meeting and/or if you have any concerns, please immediately notify the ADA Coordinator.

City Commission:
c/o Benjamin C. Marentette, CMC, City Clerk
(231) 922-4480
Email: tcclerk@traversecitymi.gov
Web: www.traversecitymi.gov
400 Boardman Avenue
Traverse City, MI 49684

The mission of the Traverse City City Commission is to guide the preservation and development of the City's infrastructure, services, and planning based on extensive participation by its citizens coupled with the expertise of the city's staff. The Commission will both lead and serve Traverse City in developing a vision for sustainability and the future that is rooted in the hopes and input of its citizens and organizations, as well as cooperation from surrounding units of government.

Welcome to the Traverse City Commission meeting!

Agenda

Any interested person or group may address the City Commission on any agenda item when recognized by the presiding officer or upon request of any Commissioner. Also, any interested person or group may address the City Commission on any matter of City concern not on the Agenda during the agenda item designated Public Comment. The comment of any member of the public or any special interest group may be limited in time. Such limitation shall not be less than five minutes unless otherwise explained by the presiding officer, subject to appeal by the Commission.

Pledge of Allegiance

1. Roll Call

2. Consent Calendar

The purpose of the consent calendar is to expedite business by grouping non-controversial items together to be dealt with by one Commission motion without discussion. Any member of the Commission, staff or the public may ask that any item on the consent calendar be removed therefrom and placed elsewhere on the agenda for full discussion and such requests will be automatically respected. If an item is not removed from the consent calendar, the action noted in parentheses on the agenda is approved by a single Commission action adopting the consent calendar.

- a. Consideration of approving minutes of the Regular Meeting of July 21, 2014. (Approval recommended) (Jered Ottenwess, Benjamin Marentette)

- b. Consideration of authorizing a purchase order for a heart monitor / defibrillator and accessories for use by the Fire Department and declaring a heart monitor / defibrillator surplus to be returned and applied as a credit for the new heart monitor/defibrillator. (Approval recommended) (Jered Ottenwess, James Tuller) (5 affirmative votes required)
- c. Consideration of authorizing a blanket purchase order for water meter endpoints for water meter reading. (Approval recommended) (Jered Ottenwess, Dave Green) (5 affirmative votes required)
- d. Consideration of authorizing a blanket purchase/service order for purchase and reconditioning of water meters. (Approval recommended) (Jered Ottenwess, Dave Green) (5 affirmative votes required)
- e. Consideration of adopting a Resolution recommending approval of a request from the Acoustic Brewing Company, LLC for a new Winery Tasting Room License, and authorizing the City Clerk to issue a registration for operation of such license at 119 North Maple Street. (Adoption and approval recommended) (Jered Ottenwess, Benjamin Marentette)
- f. Consideration of approving the Downtown Development Authority Board of Trustees' appointment of Robert Bacigalupi as Executive Director and approving the Executive Director's salary, as required by Michigan Statute, which is a housekeeping matter. (Approval recommended) (Jered Ottenwess)
- g. Consideration of authorizing a service order for the construction and testing of a fiber connection between the Water Treatment Plant and Water/Sewer Maintenance Building and a service order for the acquisition, installation and hardware programming for the new fiber connections. (Approval recommended) (Jered Ottenwess, Penny Hill) (5 affirmative votes required)
- h. Consideration of authorizing a confirming service order for emergency conduit and cable repair at the Duncan L. Clinch Marina. (Approval recommended) (Jered Ottenwess, Dave Green) (5 affirmative votes required)

- i. Consideration of declaring four trucks used by the Department of Public Services as surplus and authorizing a purchase order for the replacement trucks and the chassis, which are scheduled purchases. (Approval recommended) (Jered Ottenwess, Dave Green) (5 affirmative votes required)
- j. Consideration of authorizing a confirming purchase order for the emergency repair of the lift cylinders on the ladder truck used by the Fire Department. (Approval recommended) (Jered Ottenwess, Dave Green) (5 affirmative votes required)
- k. Consideration of authorizing an agreement to receive \$173,932 in Transportation Alternative Program Funds from the Michigan Department of Transportation for the West End Beach Trail and Access Project. (Approval recommended) (Jered Ottenwess, Timothy Lodge) (5 affirmative votes required)
- l. Consideration of authorizing a confirming service order for elevator repair service at the Old Town Parking Deck, with the repair costs reimbursed by the City's insurance carrier, less the insurance deductible. (Approval recommended) (Jered Ottenwess, Robert Bacigalupi) (5 affirmative votes required)

Items removed from the Consent Calendar

- a.
- b.
- c.

3. Old Business

- a. Consideration of enacting an amendment to the Traverse City Code of Ordinances, which make various changes to the regulations regarding sidewalk cafés (outdoor cafés), as recommended by the City Commission Ad Hoc Committee Regarding Sidewalk Cafés. (Mayor Pro Tem Carruthers, Chairman; Commissioner Barbara Budros; Commissioner Jeanine Easterday)
- b. Consideration of authorizing a contract for site investigation and schematic design, preliminary design and final design services associated with the Traverse City Public Pier Project in West Bay, as recommended by The Traverse City Public Pier Study Group. (Mayor Michael Estes, Commissioner Ross Richardson, Missy Luick)

4. New Business

- a. Consideration of authorizing a contract with Grand Traverse County for its Planning and Development Department to provide economic development services to the City and Downtown Development Authority. (Jered Ottenwess, Jean Derenzy) (5 affirmative votes required)

5. Appointments

None.

6. Reports, Announcements and Correspondence

Please note: For this section of the agenda, when an actual report is included or expected, the item will be underlined.

- a. Reports, announcements and correspondence from the City Manager.
- b. Announcements from the City Clerk.

- c. Reports, announcements and correspondence from the Mayor and City Commissioners.
- d. Reports and correspondence from other City officials, boards and committees.
 - 1. Reports from members of the Commission serving on boards.
 - 2. Quarterly Report from the Code Enforcement Officer for the period May through July 2014.
 - 3. Minutes of the Traverse City Light and Power Board meeting of June 24, 2014.
- e. Reports and correspondence from non-City officials.
 - 1. Monthly Operating Report for the Wastewater Treatment Plant from CH2M HILL for June 2014.

7. Public Comment

- a. Reserved.

Request from LaVern P. Broughton, 611 Hastings Street, regarding "Oath of Office given by the City Clerk..."

- b. General.
- c. Mayor and City Commissioners.

8. Adjournment

The mission of the Traverse City City Commission is to guide the preservation and development of the City's infrastructure, services, and planning based on extensive participation by its citizens coupled with the expertise of the city's staff. The Commission will both lead and serve Traverse City in developing a vision for sustainability and the future that is rooted in the hopes and input of its citizens and organizations, as well as cooperation from surrounding units of government.

The City of Traverse City



Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ⁵⁰JERED OTTENWESS, CITY MANAGER

SUBJECT: MINUTES

Attached are the minutes from the following meeting of the City Commission:

- July 21, 2014, Regular Meeting

The City Clerk recommends that these minutes be approved. The following motion would be appropriate:

that the minutes of the July 21, 2014, Regular Meeting, be approved.

JJO/kes

k:\tcclerk\city commission\minutes

Minutes of the
City Commission for the City of Traverse City
Regular Meeting
July 21, 2014



A regular meeting of the City Commission of the City of Traverse City was called to order at the Commission Chambers, Governmental Center, 400 Boardman Avenue, Traverse City, Michigan, at 7 p.m.

The following Commissioners were present, constituting a quorum: Mayor Michael Estes, Mayor Pro Tem James Carruthers, Barbara D. Budros, Jeanine Easterday, Gary L. Howe, Ross Richardson, Tim Werner.

The following Commissioners were absent: None.

The Pledge of Allegiance was recited.

Mayor Michael Estes presided at the meeting.

As requested by Commissioner Werner, Agenda Item 2(b) was removed from the Consent Calendar for full discussion.

As requested by Rick Buckhalter, 932 Kelley Street, Agenda Items 2(c) and 2(i) were removed from the Consent Calendar for full discussion.

2. Consent Calendar

Moved by Carruthers, seconded by Howe, that the following actions as recommended on the amended Consent Calendar portion of the Agenda be approved:

- a. the minutes of the July 7, 2014, Regular Meeting and July 14, 2014, Study Session, be approved.
- b. Removed from the Consent Calendar.

- c. Removed from the Consent Calendar.
- d. the City Manager be authorized to issue a confirming purchase/service order in the amount of \$29,494.55 to Crystal Flash Energy for 9,999 gallons of unleaded gasoline priced at \$2.94975 per gallon with funds available in the Garage Fund.
- e. the City Manager be authorized to declare old plows #275 and #283 surplus, and that the City Manager be authorized to approve a purchase order to Truck and Trailer Specialties of Boyne Falls in the amount of \$13,272, for two new Monroe MP38-64-11-RGD large front end plows, with funds available in the Garage Fund for these planned replacements.
- f. an amendment to the Traverse City Code of Ordinances, *Redevelopment Liquor License Clarification*, Section 834.06, which clarifies that all non-conflicting sections of Chapter 834 apply to redevelopment liquor licenses, which was introduced on July 7, 2014, be enacted with an effective date of July 31, 2014.
- g. the request from applicant Sandy Oliver for Special Land Use Permit 14-SLUP-01, to allow for a Group Day Care Home at 1012 Centre Street, as recommended by the City Planning Commission, provided that the general and specific standards in sections 1364.02 and 1364.08 (i) respectively are met, any violations noted by the Fire Marshal are corrected, all necessary State licenses are obtained and "Approved Child Care Provider" identification is prominently displayed in a street-side window, be scheduled for public hearing on August 18, 2014.
- h. the resolution recommending approval of a request from the Traverse City Whiskey Company, LLC, for a New Small Distillers License, be adopted; and that the City Clerk be authorized to issue a Liquor License Registration to Traverse City Whiskey Company, LLC to operate such license at 201 E. Fourteenth Street.
- i. Removed from the Consent Calendar.

- j. the Resolution Scheduling Public Hearing on Special Improvement District No. 2014-005 be adopted, scheduling the public hearing for August 18, 2014.
- k. the Resolution Scheduling Public Hearing on Special Improvement District No. 2014-01 be adopted, scheduling the public hearing for August 18, 2014.

CARRIED unanimously.

Items removed from the Consent Calendar

a.

Consideration of authorizing a service order for the 2014 Preventative Maintenance Crack Sealing Project to increase the service life of various streets, with a portion to be reimbursed by the Michigan Department of Transportation.

The following addressed the Commission:

Jered Ottenwess, City Manager

Moved by Werner, seconded by Carruthers, that the City Manager be authorized to issue a service order to Scodeller Construction, for crack sealing operations on approximately 14 miles of the City's street system and 1.2 miles of MDOT highway, in the amount of \$66,512.40, with funds available in the Capital Improvement Project Funds, and \$6,512.40 to be reimbursed by MDOT.

CARRIED unanimously.

b.

Consideration of adopting a resolution granting settlement authority to the City Manager regarding class action litigation associated with noncompetitive pricing for automotive parts.

The following addressed the Commission:

Jered Ottenwess, City Manager

Lauren Tribble-Laucht, City Attorney

Moved by Budros, seconded by Richardson, that the Resolution Granting Settlement Authority to City Manager, in connection with class action litigation regarding price fixing and anticompetitive activity associated with the prices of automotive parts, be adopted.

Rick Buckhalter, 932 Kelley Street – made general comments

CARRIED unanimously.

c.

Consideration of adopting a resolution authorizing the city to participate in the Michigan Economic Development Corporation's *Redevelopment Ready Communities Program* and authorizing the related memorandum of understanding.

The following addressed the Commission:

Jered Ottenwess, City Manager

Moved by Richardson, seconded by Budros, that the Resolution Authorizing the City of Traverse City to Approve and Fully Participate in the Michigan Economic Development Corporation (MEDC) Redevelopment Ready Communities Program and Approve the Memorandum of Understanding, be adopted.

Rick Buckhalter, 932 Kelley Street – made general comments

CARRIED unanimously.

3. Old Business

3(a).

Presentation of the report from the City Commission Ad Hoc Committee regarding Sidewalk Cafes and consideration of introducing an ordinance amendment which would make various changes to the sidewalk café ordinance;

and consideration of establishing a City Commission ad hoc committee to consider making a recommendation regarding public parklets.

The following addressed the Commission:

Jered Ottenwess, City Manager

Moved by Howe, seconded by Richardson, that an amendment to the Traverse City Code of Ordinances, *Sidewalk Café Regulation Changes*, Sections 1020.07 and 1020.08, which make various changes to the regulations regarding sidewalk cafés (outdoor cafés), as recommended by the City Commission Ad Hoc Committee Regarding Sidewalk Cafés, be introduced and scheduled for possible enactment on August 4, 2014.

CARRIED unanimously.

John McGee, 1749 East Front Street, City business owner - made general comments

3(b).

Consideration of entering into closed session to discuss the periodic personnel evaluation of City Manager Jered Ottenwess, as requested by Mr. Ottenwess.

Moved by Budros, seconded by Easterday, that the City Commission enter into closed session immediately following the public comment portion of the agenda to consider the periodic personnel evaluation of City Manager Jered Ottenwess as requested by the City Manager as allowed by MCL 15.268(a).

Roll Call:

Yes - Budros, Easterday, Howe, Werner, Carruthers, Estes.

No - Richardson.

CARRIED.

3(c).

Consideration of entering into closed session to discuss the periodic personnel evaluation of City Attorney Lauren Tribble-Laucht, as requested by Ms. Tribble-Laucht.

Moved by Budros, seconded by Werner, that the City Commission enter into closed session immediately following the public comment portion of the agenda to consider the periodic personnel evaluation of City Attorney Lauren Tribble-Laucht as requested by the City Attorney as allowed by MCL 15.268(a).

Yes - Budros, Easterday, Howe, Richardson, Werner, Carruthers, Estes.

No - None.

CARRIED unanimously.

4. New Business**4(a).**

Consideration of adopting an amendment to the Infrastructure Strategy Policy to incorporate the National Association of City Transportation Officials *Urban Street Design Guide* as a technical resource as part of the City's Infrastructure Strategy Policy and amending the policy regarding sidewalk spending.

The following addressed the Commission:

Jered Ottenwess, City Manager

Moved by Richardson, seconded by Werner, that the Infrastructure Strategy Policy be amended to adopt the Urban Street Design Guide (2013) by the National Association of City Transportation Officials as a resource document.

CARRIED unanimously.

5. Appointments

None.

6. Reports and Communications

The following were received and filed:

- a. Reports, announcements and correspondence from the City Manager.
- b. Announcements from the City Clerk.
- c. Reports, announcements and correspondence from the Mayor and City Commissioners.
- d. Reports and correspondence from other City officials, boards and committees.
 1. Reports from members of the Commission serving on boards.
 2. Quarterly Financial Report from the City Treasurer/Finance Director for the Fiscal Quarter ended March 31, 2014.
 3. Minutes of the June 3, 2014, and June 17, 2014, meetings of the Planning Commission.
 4. Minutes of the May 23, 2014, and May 29, 2014, meetings of the Historic Districts Commission.
 5. Minutes of the June 10, 2014, meeting of the Board of Zoning Appeals.
 6. Minutes of the June 11, 2014, meeting of the Traverse City Light and Power Board.
- e. Reports and correspondence from non-City officials.

- 1. Fractile Emergency Response Report from North Flight for June 2014.

7. Public Comment

The following addressed the Commission:

- 1. Reserved.

None

- 2. General.

Rick Buckhalter, 932 Kelley Street
 Charlene Hunt, 10630 South Timberlee Drive, Elmwood Township
 Dale Newman, 401 Washington Street

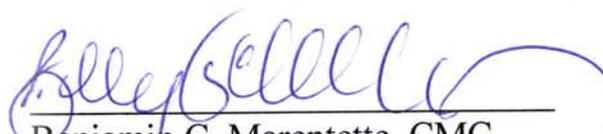
- 3. Mayor and City Commissioners.

Jered Ottenwess, City Manager

The Commission entered into closed session at 8:04 p.m.

The Commission returned to open session at 8:45 p.m.

There being no objection, Mayor Michael Estes declared the meeting adjourned at 8:46 p.m.


 Benjamin C. Marentette, CMC
 City Clerk

Approved: _____, _____
(Date) (Initials)



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ⁵⁰JERED OTTENWESS, CITY MANAGER

SUBJECT: PHYSIO CONTROL LIFEPAK 15 HEART MONITOR/
DEFIBRILLATOR AND ACCESSORIES PURCHASE

Attached is a memo from Jim Tuller, Fire Chief, requesting the purchase of a Lifepak 15 Heart Monitor/Defibrillator and accessories for use at the Fire Department. Competitive bids were not sought in this instance for the various reasons outlined in the Fire Chief's memo.

I recommend the following motion (5 affirmative votes required):

that the competitive bidding process be waived; and that the City Manager be authorized to issue a purchase order to Physio Control in the amount of \$32,082.55 to purchase a Lifepak 15 Heart Monitor/Defibrillator and accessories, with funds available in the 2014-2015 General Fund - Capital Outlay budget; and that a Lifepak 15 Heart Monitor/Defibrillator be declared surplus and returned as a credit against the new purchase.

JO/jd

cc: Jim Tuller, Fire Chief

Communication

From the Office of the Fire Chief

The City of Traverse City
Fire Department



Station 01, 500 West Front Street, Traverse City MI 49684

(231) 922-4930 Ext. 2

Friday July 25, 2014

TO: Jered Ottenwess, City Manager

REF: Physio Control Lifepak 15 Heart Monitor/Defibrillator and accessories purchase.

Jered,

The purpose of this communication is to request a "sole source" purchase of the above mentioned equipment for placement in service at the Traverse City Fire Department.

The Lifepak HMD is a unit that is used on a daily basis by our Paramedics to diagnose and treat cardiac related issues in patients that are exhibiting heart damage signs & symptoms, or have gone into cardiac arrest. It is a computer-based unit with software that enables Paramedics in the field to assess the patient's heart rhythm and send it through a cell phone modem to Munson Medical Center ER for additional review by cardiac specialists on staff.

Currently, there are 2 Lifepak 12 units in service. They are over 7 years old and nearing the end of their service life. The unit proposed is the next generation available and will fulfill the need for field diagnosis of cardiac events experienced by our residents and visitors for many years.

The request to remain with Physio Control Lifepak HMDs is based on the following:

1. Battery chargers in the fire stations and on fire apparatus are compatible, along with batteries on hand, computer/communications and cardiac pad cables/connectors.
2. EKG transmission software is compatible with Munson E.R.
3. Current Biomedical Technician used is Physio Control certified to perform annual preventive maintenance on Lifepak 12 and 15 HMDs.
4. Physio Control supplies a loaner LP unit in the event a unit is sent for service or repair.
5. TCFD personnel are trained on and familiar with the operation of the Physio Control Lifepak HMDs. Minimal training will be required to put the new unit into service.

The cost for purchase of a Lifepak 15 Heart Monitor/Defibrillator and accessories from Physio Control is \$38396.00. Less trade-in of current LP 12 and cash discount, final cost is \$32,082.55. Funding for this purchase has been secured in the 2014-2015 Capital Outlay budget. I am available to meet at any time to discuss this project.

Respectfully,

A handwritten signature in blue ink that reads "Chief Tuller". The signature is written in a cursive, flowing style.

Chief Tuller,
T.C.F.D.



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ^{JO}JERED OTTENWESS, CITY MANAGER

SUBJECT: BLANKET PURCHASE ORDER FOR WATER METERS

Attached are memos from Dave Green, DPS Director and Justin Roy, Water/Wastewater Maintenance Superintendent, regarding water meter purchasing. ETNA Supply is the only distributor of these water meters.

I recommend the following motion (5 affirmative votes required):

that the competitive bidding process be waived and that the City Manager be authorized to issue a blanket purchase order to ETNA Supply in an amount not to exceed \$80,000 for the purchase and reconditioning of water meters, for fiscal year 2014-2015, with funds available in the Water Funds.

JO/jd

cc: Dave Green, DPS Director
Justin Roy, Water/Wastewater Maintenance Superintendent

Memorandum

The City of Traverse City
Department of Public Services



TO: Jered Ottenwess, City Manager
FROM: Dave Green, DPS Director 
DATE: July 24, 2014
SUBJECT: 2014 Blanket Purchase Order for Water Meters

Attached is a memorandum from Justin Roy, Water/Wastewater Maintenance Superintendent, explaining his request for a blanket purchase order required for the purchase of large meters and the reconditioning of residential meters throughout the City. The City has standardized the use of the Sensus brand of meter and ETNA is our local distributor.

Please request City Commission approval of a blanket purchase/service order for water meters in an amount not to exceed \$80,000.00 to ETNA Supply for the purchase and reconditioning of water meters with funds available in the Water and Sewer Funds.

INTEROFFICE MEMORANDUM

TO: DAVE GREEN, DPS DIRECTOR
FROM: JUSTIN ROY, WATER/WASTEWATER MAINTENANCE SUPERVISOR 
SUBJECT: BLANKET PURCHASE ORDER FOR WATER METERS FY 1415
DATE: 7/22/2014
CC:

Every year we approach the City Commission for a blanket purchase order for water meters. Last year, we requested \$80,000 to purchase water meters from ETNA Supply. Since September of 2011, we have been installing radio read devices on the water meters, and are currently at 28% of radio reads on water meters. In addition, we continue our meter change out program.

Please request City Commission approval for a blanket purchase order for water meter purchases, in a not to exceed \$80,000 amount, with funds available in the water and sewer capital improvements budget. The City has standardized on Sensus water meters for the last 25+ years, and ETNA Supply is our only local distributor. The cost of new meters is passed along to the customer, except as part of our meter change out program. ETNA Supply gives us a discount on the price of new meters in exchange for the used meters.

Please contact me at 922-4923 if you have further questions.



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: JERED OTTENWESS, CITY MANAGER

SUBJECT: BLANKET PURCHASE ORDER FOR WATER METER
ENDPOINTS

Attached are memos from Dave Green, DPS Director and Justin Roy, Water/Wastewater Maintenance Superintendent, regarding a blanket purchase order for endpoints for water meters. United Systems and Software is the only distributor for these endpoints.

I recommend the following motion (5 affirmative votes required):

that the competitive bidding process be waived and that the City Manager be authorized to issue a blanket purchase order to United Systems & Software in an amount not to exceed \$60,000 for the purchase of water meter endpoints, for fiscal year 2014-2015, with funds available in the Water Funds.

JO/jd

cc: Dave Green, DPS Director
Justin Roy, Water/Wastewater Maintenance Superintendent

Memorandum

The City of Traverse City
Department of Public Services



TO: Jered Ottenwess, City Manager
FROM: Dave Green, DPS Director *DG*
DATE: July 24, 2014
SUBJECT: 2014 Blanket Purchase Order for Water Meter Endpoints

Attached is a memorandum from Justin Roy, Water/Wastewater Maintenance Superintendent, explaining his request for a blanket purchase order required for the purchase of endpoints for water meters. These endpoints transmit radio signals from the meter to our hand held reading devices thereby improving accuracy and efficiency in the meter reading process. This will be the fourth year in what we expect to be seven or eight year conversion project.

Please request City Commission approval of a blanket purchase order in the amount of \$60,000.00 to United Systems & Software for water meter endpoints, with funds available in the Water and Sewer Funds.



CITY OF TRAVERSE CITY
WATER/WASTEWATER MAINTENANCE

TO: DAVE GREEN, DPS DIRECTOR
FROM: JUSTIN ROY, WATER/WASTEWATER MAINTENANCE SUPERVISOR JR
SUBJECT: BLANKET PURCHASE ORDER FOR WATER METER ENDPOINTS (RADIO READS)
DATE: 7/22/2014
CC:

In 2011 the Treasurer's department bid out new readers for water and electric meters, purchasing ITRON handheld devices which have the capability to read meters by radio signal, as opposed to visually reading the meter or old style remote. The technology makes it possible to regularly read water meters that have been challenging to read on a monthly basis, and is improving the efficiency with which the meters are read, reducing the amount of water meters that are unread because of access problems.

To date we have installed roughly 2050 units, or about 28% of our system. We are once again requesting a blanket purchase order for \$60,000 from United Systems & Software, Inc., our local distributor, in order to continue our meter upgrade program.

Please request City Commission approval for a blanket purchase order for fiscal year 2014-2015 to United Systems & Software, Inc. in the amount of \$60,000, for water meter endpoint and related devices, with funds available in the water and sewer capital improvements budget. If you have any questions or concerns, please call me at 231-922-4923.



Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ^{JO}JERED OTTENWESS, CITY MANAGER

SUBJECT: LIQUOR LICENSE REQUEST – ACOUSTIC BREWING
COMPANY, LLC

Attached is a memo from City Clerk Benjamin Marentette, recommending approval of a request from the Acoustic Brewing Company, LLC, (Bruce Grossman) for a new Winery Tasting Room for operation at 119 Maple Street.

I recommend the following motion:

that the resolution recommending approval of a request from the Acoustic Brewing Company, LLC, for a New Winery Tasting Room License, be adopted; and that the City Clerk be authorized to issue a Liquor License Registration to Acoustic Brewing Company, LLC to operate such license at 119 Maple Street.

JJO/kes

K:\tcclerk\City Commission Communications\liquor license_regular\New Winery Tasting Room_Acoustic Brewing_20140804.doc

copy: Bruce Grossman, Acoustic Brewing Company

Memorandum

The City of Traverse City



TO: Jered Ottenwess, City Manager
FROM: Benjamin C. Marentette, City Clerk
DATE: Thursday, July 31, 2014
SUBJECT: Liquor License Request – Acoustic Brewing Company, LLC

The Acoustic Brewing Company, LLC, is requesting a New Winery Tasting Room License for operation at 119 Maple Street (the small brick building just off the corner of Front Street and Maple Street).

A Winery Tasting Room License is a license issued to applicants who already currently hold a Wine Maker or Small Wine Makers License to operate an off-site tasting room located away from the winery premises. The tasting room may offer free samples to consumers and may sell wine to consumers for take-out and off premise consumption.

If the City Commission adopts the resolution approving the license, then it will be provided to the Michigan Liquor Control Commission (MLCC) and it then may consider granting or not granting the license. The applicant has paid the appropriate application fee and this request has been reviewed by the appropriate city departments, including the Police Department, and meets all ordinance/law requirements. The supporting documentation is on file with this office.

This license cannot be operated unless the City Commission authorizes the City Clerk to issue a registration to the Acoustic Brewing Company, LLC. I respectfully recommend that the City Commission authorize the City Clerk to issue the registration.

As always, please let me know if you have any questions or if I may be of further assistance.

bcm/kes



Michigan Department of Licensing and Regulatory Affairs
 Liquor Control Commission (MLCC)
 7150 Harris Drive, P.O. Box 30005 - Lansing, Michigan 48909-7505
 Toll Free (866) 813-0011 • www.michigan.gov/lcc

Business ID: _____

Request ID: _____

(For MLCC use only)

Local Government Approval

(Authorized by MCL 436.1501)

Instructions for Applicants:

- Provide a copy of your Application for New Licenses, Permits, or Transfer of Ownership or Interest in License (form LCC-3011 for Retail or form LCC-3015 for Manufacturers and Wholesalers) to the local unit of government.

Instructions for Local Legislative Body:

- Complete this resolution, or provide a resolution, a letter of certification from the clerk, or minutes from the meeting at which this request was considered.

At a Regular meeting of the Traverse City council/board
(regular or special) (township, city, village)

called to order by _____ on Aug 4, 2014 at 7:00 PM
(date) (time)

the following resolution was offered:

Moved by _____ and supported by _____

that the application from Acoustic Brewing Company, LLC
(name of applicant)

for the following license(s): Winery Tasting Room
(e.g. Class C, Tavern, B-Hotel, Micro Brewer)

and the following permits, if applied for: Dance Permit Entertainment Permit Topless Activity Permit

to be located at 119 Maple Street, Traverse City, MI 49684

be considered for _____
(approval or disapproval)

Approval

Disapproval

Yeas: _____

Yeas: _____

Nays: _____

Nays: _____

Absent: _____

Absent: _____

It is the consensus of this body that it _____ this application be considered for
(recommends/does not recommend)

approval by the Michigan Liquor Control Commission.

I hereby certify that the foregoing is true and is a complete copy of th resolution offered and adopted by the Traverse City
 council/board at a Regular meeting held on Aug 4, 2014 .
(regular or special) (date) (township, city, village)

Name and title of authorized officer (please print): Benjamin C. Marentette, CMC, City Clerk

Signature and date of authorized clerk: _____

Phone number and e-mail of authorized officer: 231-922-4480, tcclerk@traversecitymi.gov



Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ^{SO}JERED OTTENWESS, CITY MANAGER

SUBJECT: DOWNTOWN DEVELOPMENT AUTHORITY – EXECUTIVE DIRECTOR

It recently came to my attention, through the Michigan Local Government Management Association List Serve, that the appointment and compensation of the Executive Director of a Downtown Development Authority must be approved by the legislative body.

As a housekeeping matter, the City Commission should approve the Downtown Development Authority Board's appointment of Mr. Bacigalupi as its Executive Director as well as the compensation. Going forward, any changes to the salary of the DDA Executive Director will need to be brought before the Commission prior to their implementation.

I recommend the following motion:

That the City Commission approves the Downtown Development Authority Board's appointment of Robert Bacigalupi as Executive Director for the Downtown Development Authority and approves the annual salary of \$70,320 to be paid to Mr. Bacigalupi.

JJO/bcm

e-copy: Steve Constantin, Downtown Development Authority Board
Chairman
Rob Bacigalupi, Downtown Development Authority Executive
Director

K:\tcclerk\city commission\employment agreements\dda bacigalupi



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 23, 2014

FROM: JERED OTTENWESS, CITY MANAGER

SUBJECT: FIBER OPTIC CABLE INSTALLATION - WATER PLANT
AND WATER/SEWER MAINTENANCE BUILDING

The City has been working on a phased project to connect all city buildings to the internet utilizing a fiber optic network provided by Traverse City Light & Power. The City entered into an agreement with TCL&P on February 16, 2010 to start the process and accomplish this.

This will be a great benefit to the City, giving the different departments the capability to share, store, and backup data. This is especially important for those departments such as Engineering, Assessing, DPS, and Water and Wastewater who use Geographic Information Systems (GIS) and Computer Aided Design (CAD).

As part of the continuing effort, the Water Fund and Sewer Fund have included the fiber connection for the Water Treatment Plant and the Water/Sewer Maintenance Building in the Six Year Capital Improvement Plan (CIP) for FY 14/15. The Water Treatment Plant currently has fiber to the building, and only needs the fiber ends connected to a switch and configured. The Water/Sewer Maintenance Building will require fiber to be run across Woodmere, under the railroad track to the building, and will also require the fiber ends to be connected to a switch and configured. Please refer to the attached memo from Larry LaCross, Asset Management/GIS Analyst.

TCL&P and Netech are sole source providers due to the proprietary nature of the system, so it is recommended that the competitive bidding process be waived in this instance.

I recommend the following motion (5 affirmative votes required):

that the City Manager be authorized to waive competitive bidding requirements as deemed in the best interests of the City and execute a service agreement for an amount not to exceed \$18,980 with Traverse City Light & Power and a service agreement for an amount not to exceed \$6,594.40 to Netch , with funds available in the Water Fund and Sewer Fund.

cc: Dave Green, DPS Director
Art Krueger, WTP Operator

Memorandum

The City of Traverse City
Department of Public Services



TO: Penny Hill, Assistant City Manager
FROM: Larry LaCross, Asset Management/GIS Analyst *LL*
DATE: July 9, 2014

SUBJECT: Install Fiber to Connect the Water/Sewer Maintenance Building and the Water Plant to the Existing Fiber Optic Network.

The purpose of this project is to install new fiber optic cable from the current Traverse City Light and Power fiber optic network to the Water/Sewer Maintenance Building (507 Hannah Avenue) and the Water Treatment Plant (2010 Eastern Avenue). This will allow the Water/Sewer Maintenance Building and the Water Treatment Plant to be on the same fiber optic network as the Governmental Center (400 Boardman Avenue), Department of Public Services (625 Woodmere Avenue), and Traverse City Light and Power (1131 Hastings Street). This will enable each computer network in the buildings above to communicate and create a centralized data source for geographic information systems (GIS), data storage, data sharing, and data backup. This project will increase efficiency and facilitate the expansion of new innovative technology for best management practices, such as; asset management, work order management, and GIS applications.

Traverse City Light and Power owns and maintains the fiber optic network that the City utilizes, while Grand Traverse County Information Technology Department (GTCITD), our IT provider, maintains the computer network that the City uses. Traverse City Light and Power services for this project are unique and can only be provided by Traverse City Light and Power, therefore we are not able to use the competitive bidding process. GTCITD services are also unique and can only be provide by Netech, an approved contractor for GTCITD. Traverse City Light and Power has provided an estimate for the construction and testing, while GTCITD has provided a quote from Netech to purchase, install, and program hardware for the new fiber connections.

Please request of the City Commission permission to issue a service order to Traverse City Light and Power in the amount of \$18,980.00 and a service order to Netech in the amount of \$6,594.40 with funds for both available in the water fund.



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: JO JERED OTTENWESS, CITY MANAGER

SUBJECT: EMERGENCY CONDUIT/CABLE REPAIR AT MARINA

Attached are memos from Dave Green, DPS Director, and Barry Smith, Clinch Marina Dockmaster, requesting a confirming service order to Windemuller Electric for emergency electrical repairs at the Duncan Clinch Marina.

I recommend the following motion (5 affirmative votes required):

That the competitive bidding process be waived; and that the City Manager be authorized to issue a confirming service order to Windemuller Electric in the amount of \$16,248.60 for emergency electrical repairs at the Duncan Clinch Marina, with funds available in the Marina Fund.

JO/jd

cc: Dave Green, DPS Director
Barry Smith, Marina Dockmaster

Memorandum

The City of Traverse City
Department of Public Services



TO: Jered Ottenwess, City Manager

FROM: Dave Green, DPS Director *DG*

DATE: July 28, 2014

SUBJECT: Emergency Conduit/Cable Repair at Duncan L. Clinch Marina

Attached is a copy of an e-mail from Barry Smith, Clinch Marina Dockmaster, explaining the timing of events necessitating my request for City Commission approval for a confirming purchase order to cover an emergency electrical system repair needed at the Marina a few weeks before the start of the Cherry Festival.

A little history first, last year we had piles of garbage at our dumpster location just south of Marina Drive because of the huge increase in waste generated by the success of the River Café as well as the addition of the Bijou By the Bay Theatre to the already heavy waste volume produced by the Marina boaters. With the Clinch Park Improvement Project transforming the area we felt it was time to enclose our dumpsters as part of the beautification process. Our Streets Department formed and poured a new concrete dumpster pad and we hired Perfect Fence to install a simple but clean looking 6 foot high galvanized fence enclosure with white privacy slats to complete the project.

As Barry explains, the fence contractor was on site digging holes for fence posts needed for the dumpster enclosure when they jack hammered through underground conduit. This caused the breakers to pop and a majority of the electrical service to the A Dock, and several boats moored there, was lost. We immediately called Windemuller Electric, the electrical contractor most familiar with our Marina and able to mobilize the quickest, to perform the emergency work. Because the jackhammer not only pierced the plastic conduit in many locations but also damaged the cable as well, Windemuller had to replace the entire run of wiring from the storage building to the junction box for A Dock, which is located west of the Harbormaster Building, north of the road. While our fence contractor did call Miss Dig for underground utility location services the conduits the contractor hit were after the meter and not part of the Miss Dig System. It was fortunate nobody was injured.

Please request City Commission approval of a confirming service order to Windemuller Electric in the amount of \$16, 248.60 for electrical repairs at Duncan Clinch Marina with funds available in the Marina Fund.



Dave Green <dgreen@traversecitymi.gov>

Clinch Marina A-Dock Power Feed Repairs

1 message

TCMarina Mailbox <tcmarina@traversecitymi.gov>

Wed, Jul 23, 2014 at 12:33 PM

To: Dave Green <dgreen@traversecitymi.gov>

On Monday afternoon June 16, 2014, Perfect Fence Company was installing a fence to enclose the trash dumpsters that service the Marina, Clinch Park, The River and The Bijou Theatre. The fence installer was using a jackhammer to break through the concrete where an end post was to be installed. The jackhammer penetrated an electric conduit containing feeder power to the Marina's A-Dock. The jackhammer cut into a feeder wire that immediately tripped a 700 amp breaker. Realizing what had happened, Matt Feenstra Service Manager for Windemuller Electric was contacted and Perfect Fence stopped installation. Power was lost to about two thirds of A-Dock leaving several boats without power.

Shortly after the incident Matt Feenstra met with Dockmaster Barry Smith to assess the damage. It was determined that more concrete had to be removed to inspect for further damage to the conduits and feeders. Service personnel from Windemuller were sent in to uncover the conduits damaged by the jackhammer. It was uncertain how long this process would take, so it was necessary to relocate boats that were without power.

After a day of chiseling out concrete, all the damaged conduits were exposed. At that time it was determined that to restore power to A-Dock it would be necessary to replace all the feeders in two conduits. Windemuller ordered the wire to replace the feeds and would begin the replacement and conduit repair as soon as the order was received. The Marina was approaching a busy weekend and the start of Cherry Festival reservations the following week. It was important to restore power as soon as possible so no further loss of business would incur.

By Thursday, June 19, 2014, new feeders were installed, conduits repaired and power was restored to A-Dock. Windemuller then covered the exposed conduits with concrete to finish the repairs.



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ⁵⁰ JERED OTTENWESS, CITY MANAGER

SUBJECT: 2014/2015 FLEET EQUIPMENT REPLACEMENT

Attached are memos from Dave Green, DPS Director, and Scott Meteer, Garage Superintendent, requesting approval to purchase two (2) new 2015 Ford F550 Truck Cab and Chassis and two (2) new 2015 Ford F350 Truck Cab and Chassis, and to declare four (4) old trucks surplus. This is a scheduled purchase.

I recommend the following motion (5 affirmative votes required):

That the City Manager be authorized to declare Trucks #49, 70, 82 and 91 surplus, and that the City Manager be authorized to approve a purchase order to Signature Ford in the amount of \$129,002.00 for the purchase of two new 2015 Ford F550 Truck Cab and Chassis and two new 2015 Ford F350 Truck Cab and Chassis, with funds available in the Garage Fund for these planned replacements.

JO/jd

cc: Dave Green, DPS Director
Scott Meteer, Garage Superintendent

Memorandum

The City of Traverse City
Department of Public Services



TO: Jered Ottenwess, City Manager
FROM: Dave Green, DPS Director *DSB*
DATE: July 28, 2014
SUBJECT: 2014/2015 Fleet Equipment Replacement, #'s 49, 70, 82 & 91

Attached is a memorandum from Scott Meteer, Garage Superintendent, requesting approval to purchase four truck cab and chassis packages. As with all our bigger trucks, we bid out the cab and chassis only and then bid, purchase and install the various attachments required by different departments once the "skeleton" truck arrives. All three vehicles have been listed for replacement in the 2014/2015 fiscal year budget recently approved.

Truck # 49 is a 1989 Ford F450 that is used by the Streets Division and will be replaced with a 2015 Ford F550, dual rear wheel 4X4 that will be equipped with a dump box. This truck is used as a construction truck during the construction season and will also be used as one of our pothole patching trucks that pulls the hot patch trailer around during pothole season.

Truck # 70 is a 1991 Chevrolet utility body truck that is used by the Streets Division and will be replaced with a 2015 Ford F550, dual rear wheel 4X4 equipped with a utility box and welder/generator unit. This vehicle is used as a crew/work truck to transport men, tools and equipment to various job sites around town.

Truck # 82 is a 2002 Ford F250 utility body truck that is used by the Water/Sewer Maintenance Division and will be replaced with a 2015 Ford F350, single rear wheel 4X4 equipped with a utility box. This vehicle is used as a crew/work truck to transport men, tools and equipment to various job sites around town.

Truck # 91 is a 1989 Ford F350 that is used by the Parks Division and will be replaced with a 2015 Ford F350, dual rear axle equipped with a dump box. We use this vehicle for hauling trash, compost, topsoil, wood chips, grass, brush and so forth and is also used to haul snow from City owned property and parking lots.

Memorandum

The City of Traverse City
Department of Public Services



Again, as happened earlier this year, current advertising and notification practice was used on this purchase request and only one bid was received for the vehicles from Signature Ford. We feel these are good competitive bids however and recommend that the vehicles be purchased from them.

Please request that the City Commission declare Truck #49, a 1989 Ford F450, Truck # 70, a 1991 Chevrolet utility truck, Truck # 82, a 2002 Ford F250 and Truck 91, a 1989 Ford F350 surplus and approve a purchase order in the amount of \$129,002.00 to Signature Ford for the purchase of two new 2015 Ford F550 Truck Cab and Chassis and two new 2015 Ford F350 Truck Cab and Chassis to be used by the Water and Sewer Maintenance Division, Parks Division and Streets Division with funds available in the Garage Fund for these needed replacements.

City of Traverse City, Michigan
INTERNAL SERVICE FUND
GARAGE FUND
2014--15 Vehicle Replacement Schedule

| Veh. # | Department | Vehicle Description | Estimated Replacement Costs |
|---|----------------|---------------------------------------|--------------------------------|
| | Police | 2008 Ford Crown Vic. | \$ 26,500 |
| | Police | Ford Crown Vic. Patrol Vehicle | 26,500 |
| | Police | Harley Davidson Patrol | 20,000 |
| | Police | Harley Davidson Patrol | 20,000 |
| | Hickory/Marina | Ford Ranger | 28,000 |
| 49 | Street | Ford F450 Dump | 60,000 |
| | Park | Chevrolet S10 Pick up | 27,500 |
| 70 | Street | Chevrolet C&C W/util. Body | 65,000 |
| | Park | Chevy Van | 75,000 |
| 82 | Water Dist. | Ford C&CF250 | 32,000 |
| 91 | Park | F350 Dump | 55,000 |
| | Street | Ford L9000 Plow/Dump | 210,000 |
| | Park | Dakota Pickup | 27,500 |
| | Street | F350 Dump Wplow | 60,000 |
| | Street | Komatsu WA 200-6 2Yd. Loader | 168,000 |
| | Street | Komatsu WA 200-6 2Yd. Loader | 168,000 |
| | Park | Bomardier Traxter (Hickory) | 13,000 |
| | Street | Holder C9700 | 176,000 |
| | Street | Caterpuillar 950H 4 Yard Loader | 260,000 |
| | TCPS | John Deere 445 | 40,000 |
| | Street | Western Star 4700 Dump, U. Body, Plow | 275,000 |
| | Street | Solartech Flashing Arrow | 10,000 |
| | Water Dist. | Sullivan Air Compressor | 25,000 |
| | Park | Bean 2020 Sprayer | 6,000 |
| | Street | Swensen Sander Body | 20,000 |
| | Street | Swensen Sander Body | 25,000 |
| | Street | Holder 50" Snow Blower | 7,500 |
| | Street | Holder 50" Snow Blower | 7,500 |
| | Street | Holder 50" Snow Blower | 7,500 |
| | Park | Holder Broom | 7,500 |
| | Street | Holder Flail Mower | 8,000 |
| | Park | Holder Flail Mower | 8,000 |
| | Street | Holder Broom | 8,000 |
| | Street | Wasau Front Plow | 9,000 |
| | Street | Gledhill Front End Plow | 9,000 |
| | Street | Asphalt Paver | 200,000 |
| Estimated Total Replacement Cost | | | \$ 2,191,000 |

Memorandum

The City of Traverse City
Department of Public Services



TO: Dave Green, DPS Director
FROM: Scott Meter, Garage Superintendent *SM*
DATE: July 28, 2014
SUBJECT: 2014 Equipment Replacement

Bids were solicited and only one was received for replacement cabs and chassis for four vehicles overdue for replacement.

- #1: #49, A 1989 F450 with a dump box is out of service. It is used by the Streets Department.
- #2: #70, a 1991 Chevrolet with a utility box is used by the Streets Department.
- #3: #82, a 2002 F250 with a utility box is used by Water Distribution.
- #4: #91, a 1989 F350 with a dump box is used by the Parks Department.

The bid from Signature Ford is as follows:

- #1: F550: \$32,545.00
- #2: F550: \$32,590.00
- #3: F350: \$33,264.00
- #4: F350: \$30,603.00

Please request of the City Commission permission to issue a purchase order to Signature Ford of Owosso in the amount of \$129,002.00

Also, please request that the Commission declare #49, 70, 82, and 91 surplus so that they may be disposed of.

This is a planned purchase and funds are available in the Garage fund.



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ^{JO}JERED OTTENWESS, CITY MANAGER

SUBJECT: EMERGENCY REPAIR FOR FIRE DEPARTMENT
LADDER TRUCK

Attached are memos from Dave Green, DPS Director, and Scott Meter, Garage Superintendent, requesting a confirming purchase order to Kovatch Mobile Equipment for emergency repairs on the Fire Department Ladder Truck.

I recommend the following motion (5 affirmative votes required):

That the competitive bidding process be waived; and that the City Manager be authorized to issue a confirming purchase order to Kovatch Mobile Equipment in the amount of \$12,191.16 for emergency repairs on two hydraulic lift cylinders for Truck #114, with funds available in the Garage Fund.

JO/jd

cc: Dave Green, DPS Director
Scott Meter, Garage Superintendent

Memorandum

The City of Traverse City
Department of Public Services



TO: Jered Ottenwess, City Manager
FROM: Dave Green, DPS Director 
DATE: July 28, 2014
SUBJECT: Emergency Repair for Fire Department Ladder Truck

Attached is a memo from Scott Meteer, Garage Superintendent, explaining his request for City Commission approval for a confirming purchase order to cover an emergency repair needed on the Fire Department KME (Kovatch Mobile Equipment) Ladder Truck.

As Scott states, one of the two hydraulic cylinders that lift the ladder on the truck is leaking hydraulic fluid and needs to be replaced. Since the cylinders work in pairs and per KME's safety requirements, if one fails, both must be replaced due to potential wear issues with the remaining cylinder.

From the time the leak was noticed until the truck is repaired, Chief Tuller has suspended any training on the aerial platform and use of the bucket/ladder is only authorized if a critical life safety/rescue situation presents itself, therefore we felt it critical to order the repair parts immediately. We did not get three quotes on the parts required for this repair as they are only available through KME.

Please request City Commission approval of a confirming purchase order to Kovatch Mobile Equipment of Nesquehoning, Pennsylvania in the amount of \$12,191.16 for two hydraulic lift cylinders for Truck # 114, the Fire Department Ladder Truck with funds available in the Garage Fund.

Cc: Jim Tuller, Traverse City Fire Chief

MEMO

To: Dave Green
From: Scott Meteer *SM*
Garage Superintendent
Subject: Emergency Repair on Ladder Truck
Date: July 28, 2014

The Fire Department ladder truck, #114, has developed a hydraulic leak on one of the cylinders that lift the entire ladder assembly. As this truck is over ten years old, for safety reasons alone, I ordered both lift cylinders. The truck manufacturer also recommended replacing them as a set.

Please request of the City Commission permission to issue a check to the builder of this unit, Kovatch Mobile Equipment of Nesquehoning PA, in the amount of \$12,191.16 for two lift cylinders.

Funds are available in the Garage Fund for this purchase.



Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM:  JERED OTTENWESS, CITY MANAGER

SUBJECT: WEST END BEACH TRAIL AND ACCESS PROJECT

Attached is a memo from City Engineer Timothy Lodge indicating the city will receive \$173,932 in Transportation Alternative Program Funds administered by the Michigan Department of Transportation for the West End Beach Trail and Access Project, and recommending that the Commission authorize the related agreement. If you would like a copy of the agreement, please let us know.

This project's estimated cost is \$389,195 – MDOT will hold the construction contract. The following are the funding sources:

| Funding source: | Funding amount: |
|---|------------------|
| Federal Transportation Alternative Program Funds (administered by MDOT) | \$173,932 |
| City funds (from Depot Property sale) | \$215,263 |
| Total: | \$389,195 |

I recommend the following motion (5 affirmative votes required):

That the Mayor and City Clerk execute a contract with the Michigan Department of Transportation for it to provide funding in the amount of \$173,932 in Federal Transportation Alternative Program Funds for the West End Beach Trail and Access Project (MDOT Project Number 116941A), with the City to designate \$215,263 in funds from the Depot Property Sale, such agreement subject to approval as to its substance by the City Manager and its

-Motion continued on following page-

form by the City Attorney; and further that a budget amendment be approved to increase both budgeted revenue and budgeted expenses by \$390,000 in the Capital Projects Fund to reflect the receipt of funds from the Michigan Department of Transportation, as well as the city's designated funds, and the subsequent expenditure.

JJO/bcm

K:\tcclerk\city commission\agreements\west end beach trail and access project mdot funding

Copy: Timothy Lodge, City Engineer

Memorandum

The City of Traverse City
Engineering Department



TO: Jered Ottenwess, City Manager

FROM: Timothy J. Lodge, City Engineer 

DATE: July 23, 2014

SUBJECT: West End Beach Trail and Access Project
Elmwood Avenue to Division Street
MDOT Project Number 116941A

We have received the Agreement from the Michigan Department of Transportation (MDOT) for construction of a non-motorized pathway along Grandview Parkway from Elmwood Avenue to Division Street. This document is MDOT's standard agreement form used on previous projects that receive federal funding. The project is currently advertised for bids with a scheduled bid opening of August 1, 2014 by the MDOT. The work is scheduled to start after September 2, 2014 and be completed by October 30, 2014. The agreement allows us to receive \$173,932 in Transportation Alternative Program funds. The project cost is estimated to be \$389,195 with \$215,263 from City funds (match from the Depot property sale).

The project is part of the Bayfront Plan and received previous approvals from the City Planning Commission on 11/16/11, Parks and Recreation Commission on 11/3/11 and by the City Commission on 11/7/11. The scope of work includes the extension of a non-motorized trail along the north side of M-72/West Grandview Parkway from the parking lot at West End Beach to Elmwood Avenue, a distance of approximately 1,100 feet; a pedestrian crossing of M-72/West Grandview Parkway near the alignment of Elmwood Avenue connecting the trail extension with neighborhoods and trail networks to the south. North of the trail crossing intersection, a small viewing platform and ramped access to the beach will be constructed; also a stormwater treatment structure will be installed to separate oil and grit street runoff and reduce this current form of point-source pollution directly into Grand Traverse Bay.

Please have the City Commission authorize the proper City officials to sign this agreement and reference the MDOT project number in the motion. It is necessary to return both signed copies, along with a certified resolution authorizing those individuals to sign the agreement to MDOT in accordance with the instructions from MDOT.

| SHEET | DESCRIPTION |
|-------|---------------------------------|
| 1 | TITLE SHEET |
| 2 | DETAIL / NOTE SHEET |
| 3 | DETAIL / NOTE SHEET |
| 4 | DETAIL / NOTE SHEET |
| 5 | PLAN SHEET #1 |
| 6 | PLAN SHEET #2 |
| 7 | PLAN SHEET #3 |
| 8 | STORMWATER TREATMENT PLAN SHEET |
| 9 | TRAFFIC MAINTENANCE SHEET |
| 10 | IRRIGATION / LANDSCAPE SHEET |
| 11 | QUANTITY SHEET |

CITY OF TRAVERSE CITY
 IN CO-OPERATION WITH
MICHIGAN DEPARTMENT OF TRANSPORTATION
 AND
FEDERAL HIGHWAY ADMINISTRATION
PLAN AND PROFILE OF PROPOSED
WEST END BEACH TRAIL AND ACCESS PROJECT
 CONTROL SECTION: STE 28420 JOB NO. 116941A
FEDERAL PROJECT #: STP **FEDERAL ITEM #:**

THE FOLLOWING ITEMS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE MDT STANDARD PLANS REFERENCED BELOW:

MDOT STANDARD PLANS - NOT PRINTED

- R-1-E DRAINAGE STRUCTURES
- R-1-E COVER K
- R-1-E COVER Q
- R-2-H SIDEWALK RAMP DETAILS
- R-23-H DRIVEWAY OPENINGS & APPROACHES, AND CONCRETE SIDEWALKS
- R-30-F CONCRETE CURBS & CONCRETE CURBS & GUTTER
- R-30-B UTILITY TRENCHES
- R-30-E SOIL EROSION & SEDIMENTATION CONTROL MEASURES

TRAFFIC & SAFETY STANDARD PLANS - NOT PRINTED

- WZD-105-A TEMPORARY GROUND DRIVEN SIGN*
- WZD-105-E TEMPORARY TRAFFIC CONTROL DEVICES*

* SPECIAL DETAIL INCLUDED IN PROPOSAL.

LOCAL UTILITIES

CITY OF TRAVERSE CITY ENGINEERING DEPARTMENT
 ADDRESS: 400 BURGESS AVENUE, TRAVERSE CITY
 TELEPHONE: (231) 922-4495

TRAVERSE CITY LIGHT AND POWER
 ADDRESS: 1371 WESTWOOD STREET, TRAVERSE CITY
 TELEPHONE: (231) 922-4444

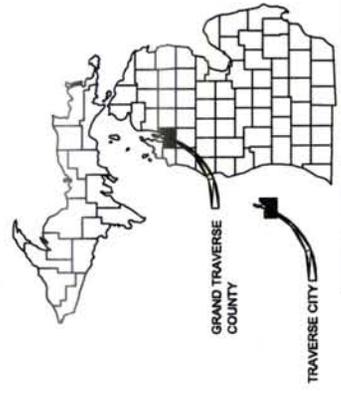
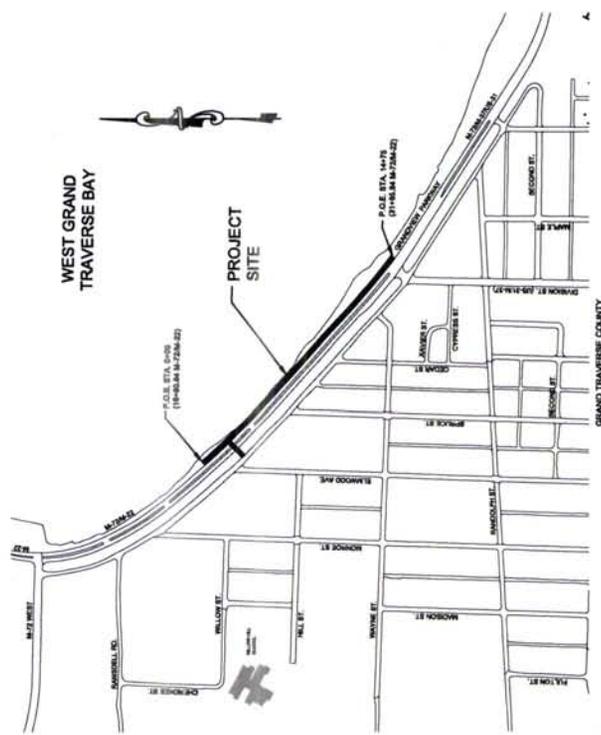
TRAVERSE CITY SOIL EROSION & SEDIMENTATION CONTROL
 ADDRESS: 400 BURGESS AVENUE, TRAVERSE CITY
 TELEPHONE: (231) 922-4495

CONCRETE EROSION CONTROL (C.E.C.)
 ADDRESS: 871 WESTWOOD ST., TRAVERSE CITY
 TELEPHONE: (231) 941-1707

ALB
 ADDRESS: 142 E STATE STREET, TRAVERSE CITY
 TELEPHONE: (231) 941-1707

MICHIGAN CONSULTING GAS COMPANY (M.C.G.)
 ADDRESS: 1200 WOODLARK E.W. P.O. BOX 274, KALAMAZOO
 TELEPHONE: (231) 948-2338

EMERGENCY / LINK RESPONSE: 1-800-647-6808



THIS PROJECT IS DESIGNED IN ACCORDANCE WITH THE 2011 MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (AMUTCD).

FOR PROTECTION OF THE UNDERGROUND UTILITIES, THE CONTRACTOR SHALL CALL 1-800-482-7171 A MINIMUM OF THREE FULL WORKING DAYS, EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS PRIOR TO BEGINNING ANY EXCAVATION WORK. THE CONTRACTOR SHALL BE ROUTINELY NOTIFIED. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF NOTIFYING UTILITY OWNERS WHO MAY NOT BE A PART OF THE "MISS DIG" ALERT SYSTEM.

THIS PROJECT IS DESIGNED IN ACCORDANCE WITH THE 2011 MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (AMUTCD).

CALL MISS DIG
 1-800-482-7171 OR 811

PROJECT LENGTH: 0.28 MILES
 CONTRACT FOR: CONCRETE SIDEWALK PATH, DRAINAGE STRUCTURES, STORMWATER TREATMENT SYSTEM, SITE FURNISHINGS, LANDSCAPING, IRRIGATION

PREPARED UNDER SUPERVISION OF

REGISTERED PROFESSIONAL ENGINEER REGISTRATION NO. 36814
 TIMOTHY J. LOOZE, P.E.

CITY APPROVAL

CITY MANAGER DATE
 JAMES O'NEILL DATE
 DEPUTY CITY MANAGER DATE
 DAVID S. COHEN DATE

1 OF 11 JOB 116941A

LOCATION MAP
 NOT TO SCALE

| SHEET | DESCRIPTION |
|-------|---------------------------------|
| 1 | TITLE SHEET |
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| 9 | TRAFFIC MAINTENANCE SHEET |
| 10 | IRRIGATION / LANDSCAPE SHEET |
| 11 | QUANTITY SHEET |

CITY OF TRAVERSE CITY
IN CO-OPERATION WITH
MICHIGAN DEPARTMENT OF TRANSPORTATION
AND
FEDERAL HIGHWAY ADMINISTRATION
PLAN AND PROFILE OF PROPOSED
WEST END BEACH TRAIL AND ACCESS PROJECT
CONTROL SECTION: TAUL 28420 JOB NO. 116941A
FEDERAL PROJECT #: _____ FEDERAL ITEM #: _____

THE FOLLOWING ITEMS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE MDOT STANDARD PLANS REFERENCED BELOW:

- MDOT STANDARD PLANS - NOT PRINTED**
- R-1-E DRAINAGE STRUCTURES
 - R-15-E COVER K
 - R-16-E COVER Q
 - R-26-H SIDEWALK RAMP DETAILS
 - R-29-H DRIVEWAY OPENINGS & APPROACHES AND CONCRETE SIDEWALKS
 - R-30-G CONCRETE CURB & CONCRETE GUTTER
 - R-33-B UTILITY TRENCHES
 - R-96-E SOIL EROSION & SEDIMENTATION CONTROL MEASURES
- TRAFFIC & SAFETY STANDARD PLANS - NOT PRINTED**
- WZD-100-A TEMPORARY GROUND DRIVEN SIGNS*
 - WZD-125-E TEMPORARY TRAFFIC CONTROL DEVICES*
 - * SPECIAL DETAIL INCLUDED IN PROPOSAL

LOCAL UTILITIES

- CITY OF TRAVERSE CITY ENGINEERING DEPARTMENT**
ADDRESS: 100 BOJORDOY AVENUE, TRAVERSE CITY, MI 49783
TELEPHONE: (231) 922-4469
- TRAVERSE CITY LIGHT AND POWER ENGINEER: BLAKE WALSON**
ADDRESS: 1131 HASTINGS STREET, TRAVERSE CITY, MI 49783
TELEPHONE: (231) 922-4944
- TRAVERSE CITY SOIL EROSION & SEDIMENTATION CONTROL CONTACT: JOHN MCWENTHY**
ADDRESS: 400 BOJORDOY AVENUE, TRAVERSE CITY, MI 49783
TELEPHONE: (231) 922-4467
- CONSULTING ENERGY COMPANY (C.P.)**
ADDRESS: 821 HASTINGS ST., TRAVERSE CITY, MI 49783
AREA ENGINEER: GREG MORTENSEN
TELEPHONE: 1-800-477-5690
- AVIAT**
ADDRESS: 142 E STATE STREET, TRAVERSE CITY, MI 49783
ENGINEER: JEFFREY STAMBERGER
TELEPHONE: (231) 941-2707
- MICHIGAN CONSOLIDATED GAS COMPANY (M.C.G.)**
ADDRESS: 1260 MICHIGAN LANE, S.W. P.O. BOX 278, KALASKA, MI 49753
AREA ENGINEER: MATT LOGAN
TELEPHONE: (231) 288-3786
EMERGENCY LEAK RESPONSE: 1-800-947-5900



LOCATION MAP

NOT TO SCALE



THE IMPROVEMENT COVERED BY THESE PLANS SHALL BE DONE IN ACCORDANCE WITH THE MICHIGAN DEPARTMENT OF TRANSPORTATION CURRENT STANDARD SPECIFICATIONS AND SUPPLEMENTAL SPECIFICATIONS, 2012 EDITION.

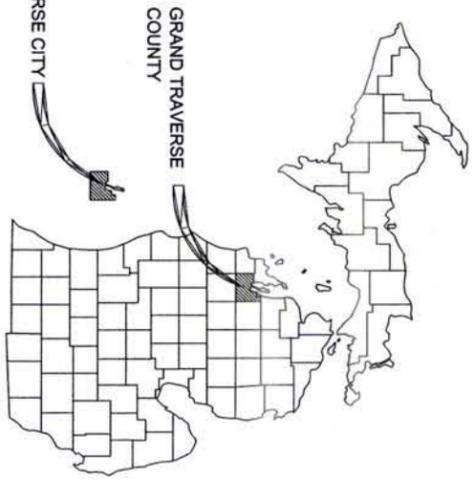
THIS PROJECT IS DESIGNED IN ACCORDANCE WITH AASHTO GUIDE FOR THE PLANNING, DESIGN, AND OPERATION OF PEDESTRIAN FACILITIES, 2004.

CALL MISS DIG
1-800-482-7171 OR 811

FOR PROTECTION OF THE UNDERGROUND UTILITIES, THE CONTRACTOR SHALL CALL 1-800-482-7171 A MINIMUM OF THREE FULL WORKING DAYS, EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS PRIOR TO BEGINNING CONSTRUCTION OPERATIONS. MEMBERS WILL BE ROUTINELY NOTIFIED. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF NOTIFYING UTILITY OWNERS WHO MAY NOT BE A PART OF THE MISS DIG ALERT SYSTEM.

THIS PROJECT IS DESIGNED IN ACCORDANCE WITH THE 2011 MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MAMUTCOD).

ADT (2014) = 22,800
ADT (2034) = 25,080
% COMM (2014) = 1%
% COMM (2034) = 1%
POSTED SPEED = 35 MPH
DESIGN SPEED = 35 MPH



PROJECT LENGTH: 0.28 MILES
CONTRACT FOR: CONCRETE SIDEWALK, DRAINAGE STRUCTURES, STORMWATER TREATMENT SYSTEM, SITE FURNISHINGS, LANDSCAPING, IRRIGATION

PREPARED UNDER SUPERVISION OF



REGISTERED PROFESSIONAL ENGINEER
TIMOTHY J. LOOPE, P.E.
REGISTRATION NO. 3614

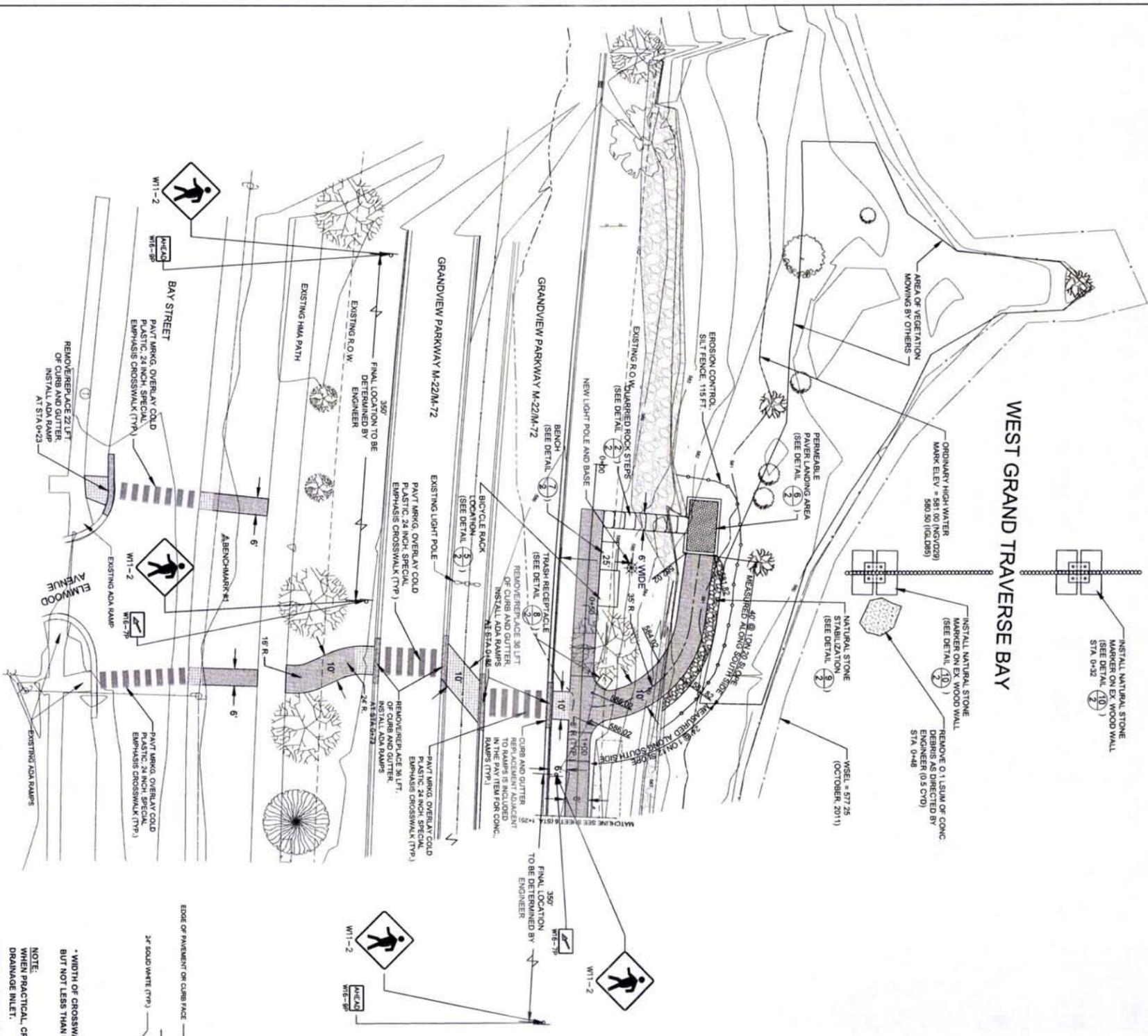
CITY APPROVAL

David Ottaway
CITY MANAGER
JERED OTTENWISS
DPS DIRECTOR
DAVID S. GREEN

5-14-14
DATE

5/14/14
DATE

WEST GRAND TRAVERSE BAY



QUANTITY DESCRIPTION THIS SHEET

| ITEM DESCRIPTION | UNIT | QUANTITY |
|---|------|----------|
| MONUMENTATION | LS | 1 |
| CURB AND GUTTER, REM | FT | 64 |
| QUARRIED ROCK STEPS | LF | 19 |
| NATURAL ROCK STABILIZATION (6x12 INCHES) | LF | 30 |
| BICYCLE RAMP W/ CONC PAD | EA | 1 |
| SIDEWALK CONC. 6 INCH | SF | 2131 |
| DETECTABLE WARNING SURFACE CAST IRON | SF | 598 |
| PAVING ONLY COLD PLASTIC 24 INCH SPECIAL EMPHASIS CROSSWALK | FT | 198 |
| SHARED USE PATH GRADING | EA | 1 |
| PERMEABLE PAVED LANDSCAPE | EA | 1 |
| LIGHTING SYSTEM | EA | 1 |
| WASTE / RECYCLING RECEPTACLE W/ CONC PAD | EA | 1 |
| BENCH W/ CONC PAD | EA | 1 |
| NATURAL STONE MARKERS | EA | 2 |
| EROSION CONTROL, SILT FENCE | LF | 118 |
| CONC. DEBRIS REMOVE | LSM | 0.1 |
| POST, STEEL, 3/8 | LF | 112 |
| SIGN, TYPE III B | EA | 44 |

LEGEND

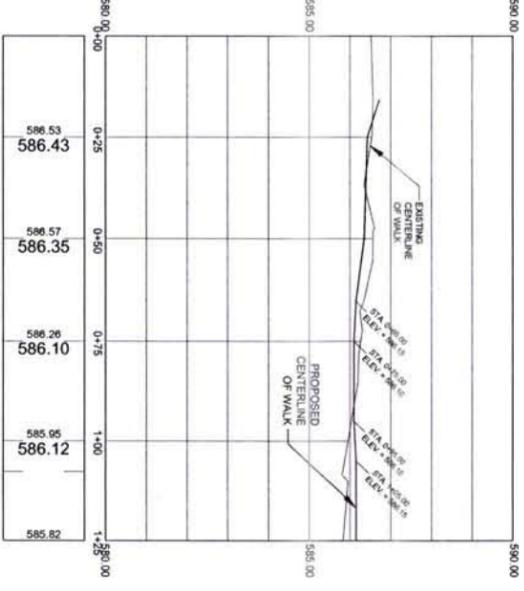
- ✱ FOUND PROPERTY CORNER
- ▲ BENCHMARK
- SIGN
- ⊠ GAS BOX
- ⊡ UTILITY POLE
- ⊢ GUY WIRE
- ⊣ WATER VALVE
- ⊤ PED LIGHT POLE
- ⊥ ELECTRIC RISER
- ⊦ TELEPHONE RISERBOX
- ⊧ FIRE HYDRANT
- ⊨ CATCHBASIN
- ⊩ BEEMVE GRATE CATCHBASIN
- ⊪ STORM MANHOLE
- ⊫ SANITARY MANHOLE
- ⊬ DECIDUOUS TREE
- ⊭ CONIFEROUS TREE
- ⊮ TREE LINE

HATCH LEGEND

- PROPOSED SIDEWALKS CONC. 6"
- PROPOSED ADA RAMP 6" CONC. W/ DETECTABLE WARNING
- CL CURB & GUTTER REMOVE / REPLACE

A BENCHMARK DATA
 BENCHMARK POINT
 CORNER POINT
 ELEV. = 586.82

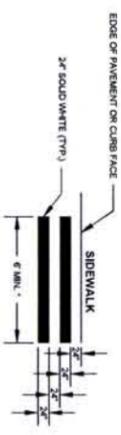
SCALE: 1" = 20'



SCALE: H, 1" = 20' ; V, 1" = 2'

SIGN, TYPE III B, PLAN QUANTITIES

| SYMBOL | LABEL | DIMENSION (ft) | AREA (sq ft) | QTY | SFT |
|--------------|--------|----------------|--------------|-----|-----------|
| | W11-2 | 3x3 | 9 | 4 | 36 |
| | W16-7P | 1x2 | 2 | 2 | 4 |
| | W16-9P | 1x2 | 2 | 2 | 4 |
| TOTAL | | | | | 44 |



NOTE:
 WIDTH OF CROSSWALK TO EQUAL WIDTH OF THE ADJACENT ADA RAMP,
 BUT NOT LESS THAN 6 FEET.

NOTE:
 WHEN PRACTICAL, CROSSWALK LOCATION SHOULD AVOID CONFLICT WITH
 DRAINAGE INLET.

**SPECIAL EMPHASIS CROSSWALK PAVEMENT MARKINGS
 NOT TO SCALE**

NOTE:
 WATER LEVEL OF WEST GRAND TRAVERSE BAY IS
 SUBJECT TO CHANGE. THE CONTRACTOR IS
 RESPONSIBLE FOR MAKING A DETERMINATION OF
 WATER LEVELS THAT MAY EXIST DURING
 CONSTRUCTION.

STATION EQUATION: PROJECT STATION 0+00 IS
 STATION 16+80.84 M-22M72

**CITY OF TRAVERSE CITY
 ENGINEERING DEPARTMENT
 400 BOARDMAN AVE
 TRAVERSE CITY, MI 49684**

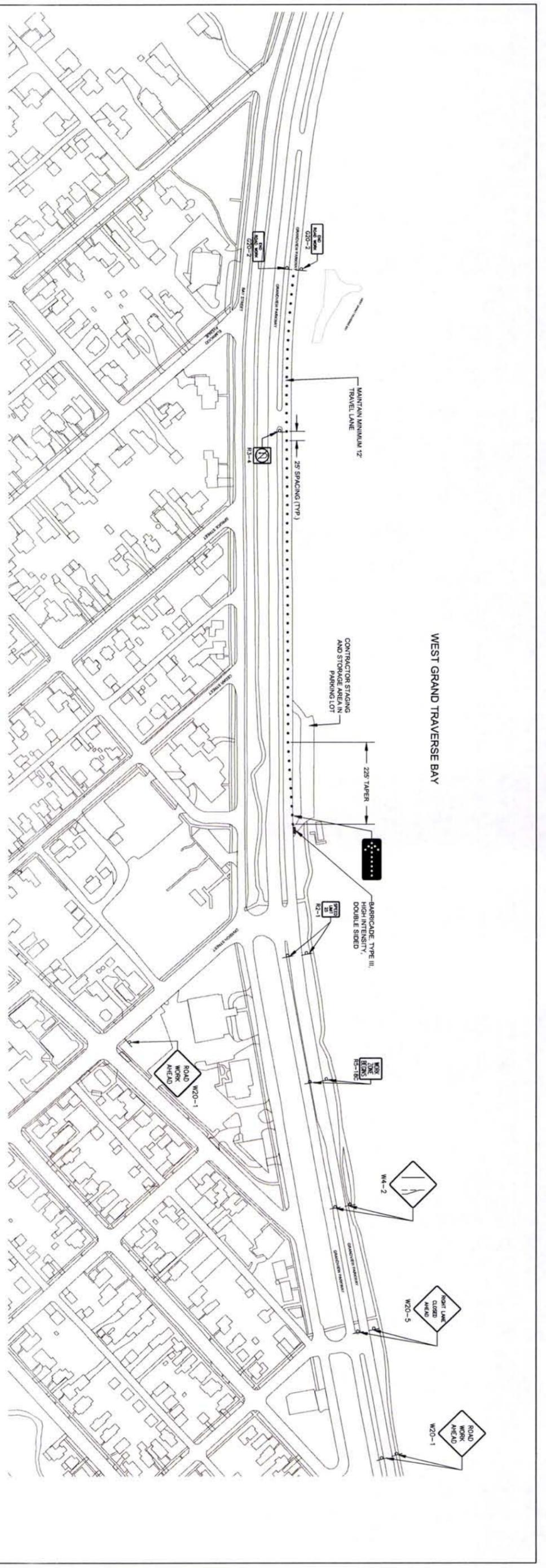
DRAWING TITLE
 WEST END BEACH TRAIL AND ACCESS PROJECT
 PLAN AND PROFILE SHEET
 P.O.B. TO STA. 1+25

| | | | |
|-----|----------------------|----|------|
| NO. | REVISION DESCRIPTION | BY | DATE |
| 2 | REVISED SUBMITTAL | | |
| 1 | G.I. SUBMITTAL | | |

SCALES:
 VERTICAL: N/A
 HORIZONTAL: 1" = 20'

DATE: DECEMBER 2011
 DESIGNED: T.L.
 DRAWN: J.M.
 APPROVED: T.L.

SHEET 5
 OF 11 SHEETS



SIGN TYPE B, TEMP. PLAN QUANTITIES

| ITEM DESCRIPTION | UNIT | QUANTITY |
|--|------|----------|
| BARRICADE TYPE III, HIGH INTENSITY, DOUBLE SIDED, LIGHTED FURN | EA | 1 |
| BARRICADE TYPE III, HIGH INTENSITY, DOUBLE SIDED, LIGHTED OPER | EA | 1 |
| MINOR TRAIL DEVICES | LS | 1 |
| PLASTIC DRUM, HIGH INTENSITY, FURN | EA | 75 |
| PLASTIC DRUM, HIGH INTENSITY, OPER | EA | 75 |
| SIGN TYPE B, TEMP PRISMATIC, FURN | SFT | 205.00 |
| SIGN TYPE B, TEMP PRISMATIC, OPER | SFT | 205.00 |
| LIGHTED ARROW, TYPE C, FURN | EA | 2 |
| LIGHTED ARROW, TYPE C, OPER | EA | 2 |
| TRAFFIC REGULATOR CONTROL | LS | 1 |

MAINTAINING TRAFFIC NOTES:

1. THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN ALL NECESSARY BARRICADES, SIGNS AND OTHER NECESSARY TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE 2011 MUTCD AND THE 2012 STANDARD SPECIFICATIONS FOR CONSTRUCTION.
2. TRAFFIC SHALL BE MAINTAINED TO ALL LOCAL RESIDENCES AND BUSINESSES AT ALL TIMES.
3. ALL WARNING SIGNS SHALL BE TYPE B, TEMPORARY FLEXIBLE ROLL UP SIGNS WILL NOT BE ALLOWED.
4. IT IS ANTICIPATED THAT CONSTRUCTION STAGING WILL COMMENCE AS SOON AS PERMITTED BY THE LOCAL PERMITTING AGENCIES TO THE CONSTRUCTION STAGING SHALL BE APPROVED BY THE ENGINEER.
5. MAINTAIN PEDESTRIAN FACILITIES USING EXISTING SIDEWALKS, PROPOSED SIDEWALKS OR TEMPORARY WOOD SIDEWALKS AS DIRECTED BY ENGINEER.
6. ALL SIGNS SHALL BE PLACED IN LOCATIONS SUCH THAT A MINIMUM 3 FT CLEAR PATH ON ALL SIDEWALKS IS MAINTAINED.
7. WORK HOURS FOR LANE CLOSURE SHALL BE IN ACCORDANCE WITH THE MOOT PERMIT FOR THE PROJECT. OVERNIGHT LANE CLOSURES ARE PROHIBITED.
8. SEE THE PROPOSAL FOR MOOT TYPICALS M0220a, M0070a, M0080a, M0110a, M0730a, M0080a AND STANDARD PLANS WZD-100-4 AND WZD-125E.

| SYMBOL | LABEL | DIMENSION (H) | AREA (SQ FT) | QTY | SFT |
|--------|--------|---------------|--------------|-----|-----|
| | W4-2 | 3x3 | 9 | 4 | 36 |
| | W4-1 | 3x3 | 9 | 4 | 36 |
| | W03-1 | 3x3 | 9 | 4 | 36 |
| | W03-5 | 3x3 | 9 | 4 | 36 |
| | W03-4 | 3x3 | 9 | 4 | 36 |
| | W03-1C | 4x5 | 20 | 4 | 80 |
| | R0-1 | 2x15 | 5 | 4 | 20 |
| | G03-2 | 3x15 | 4.5 | 4 | 18 |
| | R3-4 | 2x2 | 4 | 2 | 8 |
| TOTAL | | | | | 360 |

CONSTRUCTION STAGING PHASES:

- STAGE 1 MAINTENANCE OF TRAFFIC**
 DAYTIME CLOSURE OF THE NORTH LANE OF THE WESTBOUND TRAVEL LANES AND CONSTRUCTION WORK IN ACCORDANCE WITH THE PLANS AS SHOWN ABOVE
- STAGE 2 MAINTENANCE OF TRAFFIC (NOT SHOWN)**
 DAYTIME CLOSURE OF THE SOUTH LANE OF THE WESTBOUND TRAVEL LANES TO COMPLETE WORK ON THE SOUTH SIDE IN ACCORDANCE WITH THE PLANS USING MOOT TYPICAL M0730a. STAGE 3 CAN BE CONDUCTED WITH STAGE 1.
- STAGE 3 MAINTENANCE OF TRAFFIC (NOT SHOWN)**
 DAYTIME CLOSURE OF THE SOUTH LANE OF THE EASTBOUND TRAVEL LANES TO COMPLETE WORK ON THE SOUTH SIDE IN ACCORDANCE WITH THE PLANS USING MOOT TYPICAL M0730a. STAGE 3 CAN BE CONDUCTED WITH STAGE 1.
- STAGE 4 MAINTENANCE OF TRAFFIC (NOT SHOWN)**
 CLOSURE ON THE NORTH EDGE OF BAY STREET TO CONSTRUCT THE MODIFICATIONS TO THE STORM SYSTEM NEAR SPRUCE STREET USING MOOT TYPICAL M0710a.
- STAGE 5 MAINTENANCE OF TRAFFIC (NOT SHOWN)**
 LANE CLOSURES USING THE APPLICABLE TYPICAL MOOT MAINTENANCE OF TRAFFIC (MOT) PLAN FOR PLACEMENT OF PAVEMENT MARKINGS

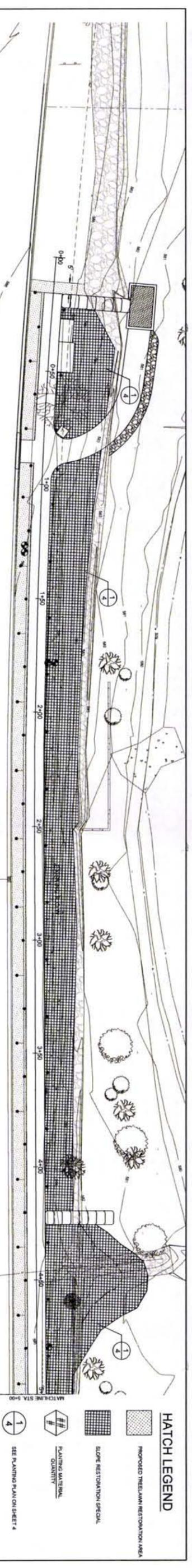


SCALE: 1"=80'

LEGEND

| | |
|--|------------------------------|
| | BARRICADE |
| | TYPE III, LIGHTED |
| | PLASTIC DRUM, HIGH INTENSITY |
| | SIGN TYPE B, TEMP INTENSITY |
| | LIGHTED ARROW DISPLAY |

| | | |
|--|--|------------------------------------|
| <p>CITY OF TRAVERSE CITY ENGINEERING DEPARTMENT 400 BOARDMAN AVE. TRAVERSE CITY, MI, 49684</p> | | <p>NO. 1</p> <p>DATE: 11-23-12</p> |
| <p>DRAWING TITLE WEST END BEACH TRAIL AND ACCESS PROJECT TRAFFIC MAINTENANCE PLAN</p> | | <p>T.L. 4-24-14</p> |
| <p>SCALES: VERTICAL: N/A HORIZONTAL: 1"=80'</p> <p>DATE: DECEMBER 2011 DESIGNED: T.L. DRAWN: JOM APPROVED: T.L.</p> | | <p>T.L. 11-23-12</p> |
| <p>SHEET 9</p> | | <p>DATE:</p> |
| <p>JOB NO. 116R41A</p> | | <p>OF 11 SHEETS</p> |

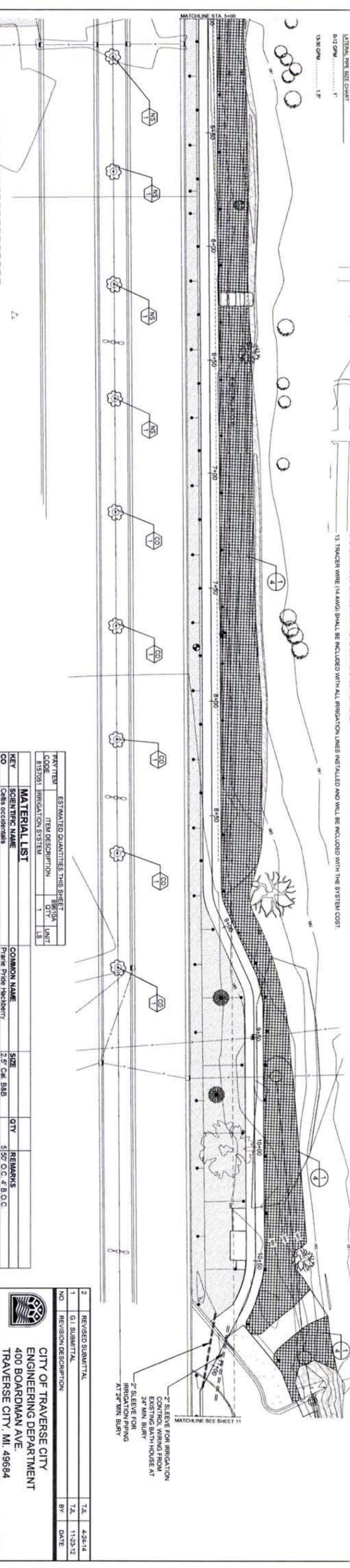


IRRIGATION LEGEND

- 7" POP UP SPRAY W/ WIRE NOZZLE
- GARDY COUPLING VALVE, 2"
- ⊕ ELECTRIC VALVE, 2" WITH 0-1 LATCHING SOLENOID
- ⊖ AUTOMATIC CONTROLLER
- ⊞ WATER FIT
- TAP LOCATION, 2"
- ▲ PRESSURE VACUUM BREAKER, 2"
- PVC MAINLINE, CLASS 160, 8" B.S.P.T. SIZE 2"
- POLY LATERAL, 100 PPI, NSF, 1/2" BURST, SEE PIPE CHART
- PVC SLEEVING, CLASS 160, BE
- EXISTING WATERMAIN
- PROPOSED 2" WATER SERVICE
- LATERAL PIPE SIZE CHART
- 0.2 GPM 1"
- 13.50 GPM 1.5"

IRRIGATION SPECIFICATIONS:

1. ALL WORK IS TO BE IN COMPLIANCE WITH ALL LOCAL, STATE AND FEDERAL CODES AND ORDINANCES. SEE THE PROPOSAL FOR ADDITIONAL DETAIL.
2. CONTROLLER TO BE FULLY WATERPROOF, PROGRAMMABLE, BATTERY-OPERATED LINE OF DURABLE CONTROLLERS AND SHALL BE RAINBIRD HUNTER TONK OR APPROVED EQUAL.
3. ALL UNDERGROUND ELECTRICAL CONNECTIONS ARE TO BE MADE WITH 3/4" WIRE CONNECTORS, ONLY.
4. ALL AUTO CONTROL VALVES ARE TO BE INSTALLED IN CARSON VALVE BOXES OF APPROPRIATE SIZE.
5. ALL CONTROL WIRING DOWNSTREAM OF THE CONTROLLER IS TO BE PER MANUFACTURERS SPECIFICATION.
6. ALL ROTONS AND SPRAY POP-UPS SHALL BE INSTALLED ON SWING PIPE.
7. ALL PVC SHALL BE INSTALLED ON 1.5 ELBOW PVC SWING JOINTS.
8. SYSTEM DESIGN BASED UPON 22 GPM @ 30 PSI.
9. ANY CHANGES IN AVAILABILITY OF SUPPLY, SHOULD BE NOTED AND MODIFICATIONS TO THE DESIGN SHOULD BE MADE.
10. CONTRACTOR TO VERIFY WATER PRESSURE AND AVAILABILITY PRIOR TO INSTALLATION.
11. ANY IRRIGATION PIPING SHOWN OUTSIDE OF CURBS FOR CLARITY ONLY.
12. IRRIGATION SYSTEM INCLUDES ALL WORK, MATERIALS AND LABOR INCLUDING WATER SERVICES TO PROVIDE COMPLETE AND OPERATIONAL SYSTEM IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
13. TRACER WIRE (14 AWG) SHALL BE INCLUDED WITH ALL IRRIGATION LINES INSTALLED AND WILL BE INCLUDED WITH THE SYSTEM COST.



GENERAL PLANTING NOTES:

1. CONTRACTOR IS RESPONSIBLE FOR REMOVAL OF ANY EXISTING GRASS WEEDS OR SCRUB GROWTH WITHIN LIMITS OF PLANT BED EDGE ON WITHIN 36" DIAMETER CIRCLE AROUND BASE OF EACH TREE.
2. PLANT SO THAT TOP OF ROOT BALL IS EVEN WITH THE FINISH GRADE ON EXISTING SOIL BASE. FOR BACKFILL AREAS PLANT BALL UP TO 2-1/2" 3" HIGHER TO ALLOW FOR SETTLEMENT.
3. WHERE PRUNING IS REQUIRED PRUNE ALL CUTS OVER 2" DIA.
4. FLAG GUY WIRES WITH SURVEYOR TAPE FOR TREES.
5. CONTRACTOR MAY DELETE STAKING OF PLANT MATERIAL IN AREAS ADJACENT TO PEDESTRIAN PATHWAYS PER ENGINEERS APPROVAL.

GENERAL SEEDING NOTES:

1. THIS STANDARD ILLUSTRATES THE TYPICAL USE OF SEEDING WITH MULCH. AS THESE ITEMS RELATE TO ROADWAY CONSTRUCTION, THE ACTUAL DESIGN AND MATERIALS USED TO CONSTRUCT THE COMPLETE SECTION WHICH INCLUDES SEEDING WITH MULCHING, WILL BE ACCORDING TO THE PLANS AND CURRENT SPECIFICATIONS.
2. ITEMS CALLED FOR ON THIS SHEET MAY ALSO BE USED DURING CONSTRUCTION AS AN EROSION CONTROL MEASURE. SEE STANDARD PLAN R-66-SERIES.
3. PERENNIALS ARE TO BE FULLY DEVELOPED TWO YEAR IN CONTAINER PLANTS.
4. ENTIRE PERENNIAL BED SHALL BE EXCAVATED DOWN 6" AND REPLACED WITH 6" OF PREPARED SOIL.

| ITEM CODE | ITEM DESCRIPTION | QUANTITY | UNIT |
|-----------|-------------------|----------|------|
| 8157051 | IRRIGATION SYSTEM | 1 | LS |

| KEY | SCIENTIFIC NAME | COMMON NAME | SIZE | QTY | REMARKS |
|-----|--|-----------------------|---------------|--------|------------------------------------|
| CO | Celtis occidentalis | Prune Prick Hackberry | 2.5" Cal. B&B | 5150 | O.C. 4' B.O.C. |
| GT | Gleditsia triacanthos inermis | Sourwood Honeylocust | 2.5" Cal. B&B | 8150 | O.C. 4' B.O.C. |
| NS | Nyssa sylvatica | Black Gum | 2.5" Cal. B&B | 5150 | O.C. 4' B.O.C. |
| AB | Arthropodium brevifolium | American Beach Grass | Plugs | 6197 | Slipper plants @ 18" O.C. |
| AC | Allium centurium | Knocking Pink Onion | Plugs | 2063 | Slipper plants @ 18" O.C. |
| CL | Cortopsis lanceolata | Lancelot's Cortopsis | Plugs | 2063 | Slipper plants @ 18" O.C. |
| | TOPSOIL SURFACE FURNISH & LUNCH | | STD | 1339 | 631225# Per Acre |
| | FERTILIZER, CHEMICAL NUTRIENT, CLASS A | | LB | 61220# | Per Acre |
| | SEEDING MIXTURE, TUF | | LB | 3211 | Per Acre |
| | MULCH BLANKET | | STD | 1872 | Between sidewalks and TOE of stone |

ESTIMATED QUANTITIES THIS SHEET

REVISIONS:

| NO. | REVISION DESCRIPTION | BY | DATE |
|-----|----------------------|----|----------|
| 1 | G.I. SUBMITTAL | | 11-23-12 |
| 2 | REVISED SUBMITTAL | | 4-24-14 |

CITY OF TRAVERSE CITY
ENGINEERING DEPARTMENT
400 BOARDMAN AVE.
TRAVERSE CITY, MI, 49684

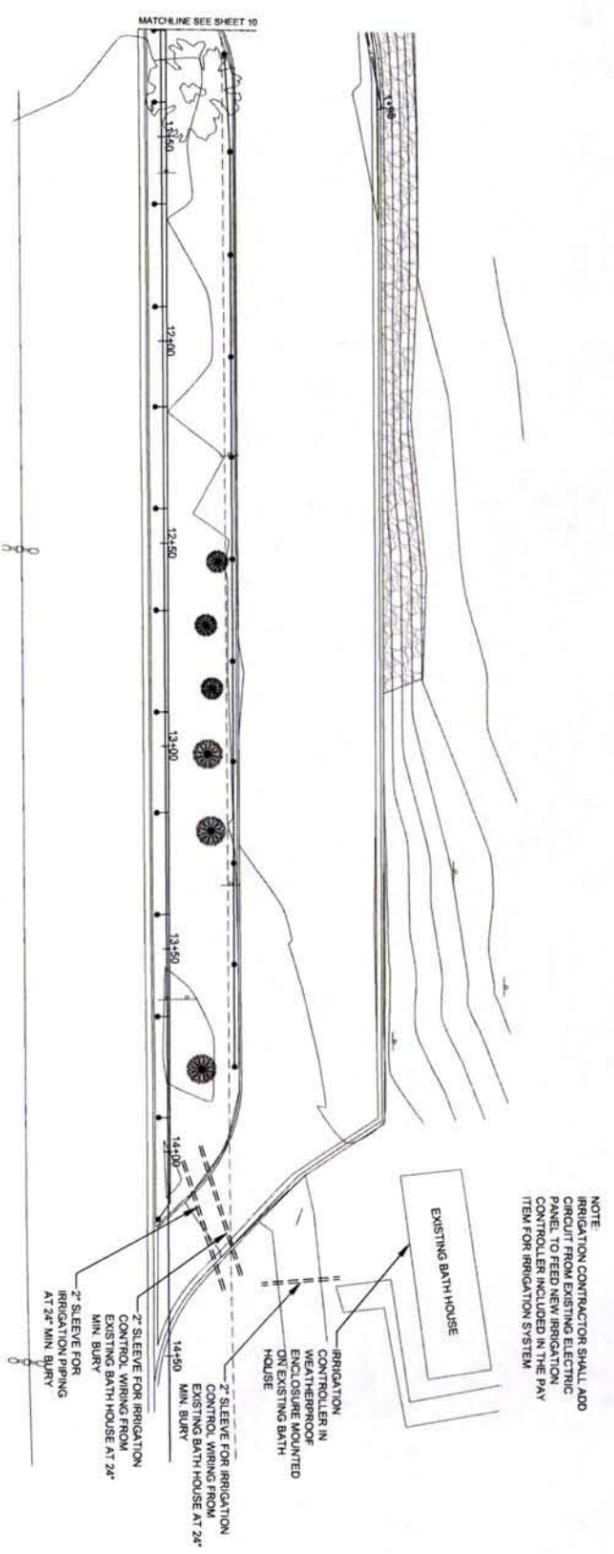
DRAWING TITLE:
WEST END BEACH TRAIL AND ACCESS PROJECT
IRRIGATION AND LANDSCAPING PLAN

SCALES:
 VERTICAL: 1"=20'
 HORIZONTAL: AS NOTED

DATE: DECEMBER 2011
 DESIGNED: T.L.
 DRAWN: J.M.
 APPROVED: T.L.

JOB NO. 119414

SHEET 10
 OF 11 SHEETS



NOTE:
IRRIGATION CONTRACTOR SHALL ADD CIRCUIT FROM EXISTING ELECTRICAL PANEL TO THE IRRIGATION CONTROL SYSTEM. IRRIGATION CONTROL SYSTEM SHALL BE INCLUDED IN THE PAY ITEM FOR IRRIGATION SYSTEM.

IRRIGATION LEGEND

- 4" POP UP SPRINKLER WITH NOZZLE
 - QUICK-CLOSURE VALVE, 2"
 - ⊙ ELECTRIC VALVE, 2" WITH O.C. LATCHING SOLENOID
 - ⊠ AUTOMATIC CONTROLLER
 - METER PIT
 - TAP LOCATION, 2"
 - ▼ PRESSURE VACUUM BREAKER, 2"
 - PVC MAINLINE CLASS 160, 11" BURY, SIZE 7"
 - PVC LATERAL, 100 PSI, 1/2" BURY, SEE PIPE CHART
 - PVC SLEEPING CLASS 160, 8"
 - EXISTING WATERMAIN
 - PROPOSED 2" WATER SERVICE
- LATERAL PIPE SIZE CHART
 0-1/2" GPM 1"
 1-3/4" GPM 1 1/2"

SCALE: 1"=20'

IRRIGATION SPECIFICATIONS

1. ALL WORK IS TO BE IN COMPLIANCE WITH ALL LOCAL, STATE AND FEDERAL CODES AND ORDINANCES. SEE THE PROPOSAL FOR ADDITIONAL DETAIL.
2. CONTROLLER TO BE FULLY WATERPROOF, PROGRAMMABLE, BATTERY-OPERATED LINE OF BURIAL CONTROLLERS AND SHALL BE RAINBIRD, HUNTER, TORO OR APPROVED EQUAL.
3. ALL UNDERGROUND ELECTRICAL CONNECTIONS ARE TO BE MADE WITH 3M WIRE CONNECTORS, DBY.
4. ALL AUTO CONTROL VALVES ARE TO BE INSTALLED IN CASCON VALVE BOXES OF APPROPRIATE SIZE.
5. ALL CONTROL WIRING DOWNSTREAM OF THE CONTROLLER IS TO BE PER MANUFACTURERS SPECIFICATION.
6. ALL ROTONS AND SPRAY POP-UPS SHALL BE INSTALLED ON SWING PIPE.
7. ALL OCV SHALL BE INSTALLED ON 3/4" ELBOW PVC SWING JOINTS.
8. SYSTEM DESIGN BASED UPON 22 GPM @ 30 PSI.
9. ANY CHANGES IN AVAILABILITY OF SUPPLY SHOULD BE NOTED AND MODIFICATIONS TO THE DESIGN SHOULD BE MADE.
10. CONTRACTOR TO VERIFY WATER PRESSURE AND AVAILABILITY PRIOR TO INSTALLATION.
11. ANY IRRIGATION PIPING SHOWN OUTSIDE OF CURBS FOR CLARITY ONLY.
12. IRRIGATION SYSTEM INCLUDES ALL WORK, MATERIALS AND LABOR INCLUDING WATER SERVICES TO PROVIDE COMPLETE AND OPERATIONAL SYSTEM IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
13. TRACER WIRE (14 AWG) SHALL BE INCLUDED WITH ALL IRRIGATION LINES INSTALLED AND WILL BE INCLUDED WITH THE SYSTEM COST.

| PartItemCode | Description | Units | Participating Qty | Non-Participating Qty |
|--------------|--|-------|-------------------|-----------------------|
| 1500001 | Mobilization, Max. | LS | 1 | |
| 200004 | Tree Rem, 6inch to 18inch | EA | 3 | |
| 200020 | Curb and Gutter, Rem | EA | 108 | |
| 2047011 | Part. Rem. Special | Syd | 89 | |
| 2047051 | Conc. Debris, Remove | LS | 1 | |
| 2047051 | Cut-Off/Remove Wood Seawall | LS | 1 | |
| 2080098 | Erosion Control, Silt Fence | EA | 319 | |
| 3020020 | Aggregate Base, 8inch | Syd | 39 | |
| 3027011 | Aggregate Base Under Grassed Area | Syd | 28 | |
| 400004 | Sewer, C.I.A., 12 inch, T' Det A | EA | 15 | |
| 4020007 | Sewer, C.I.A., 24 inch, T' Det A | EA | 52 | |
| 4021204 | Sewer Tap, 12 inch | EA | 1 | |
| 4027001 | Sewer, Perforated, 12 inch, Stone Trench | EA | 120 | |
| 4030005 | Dp Structure Cover, Adj. Cse 1 | EA | 2 | |
| 4030050 | Dp Structure Cover, Type K | EA | 1 | |
| 4030055 | Dp Structure Cover, Type Q | EA | 5 | |
| 403010 | Dp Structure, 48 inch dia | EA | 1 | |
| 4030220 | Dp Structure, 60 inch dia | EA | 3 | |
| 4030230 | Dp Structure, 72 inch dia | EA | 1 | |
| 4037050 | Dp Structure, 72 inch dia, Access Hatch, TCOG | EA | 1 | |
| 4037050 | Dp Structure, 72 inch dia, Access Hatch, Wier | EA | 1 | |
| 4037050 | Stormwater Treatment System | EA | 1 | |
| 5001033 | HMA, 13A | Ton | 5 | |
| 8000023 | Curb and Gutter, Conc., Det C4 | EA | 33 | |
| 8000036 | Sidewalk Ramp, Conc, 6inch | EA | 508 | |
| 8000046 | Sidewalk, Conc, 6inch | EA | 10606 | |
| 8037001 | Detachable Warning Surface, Cast Iron | EA | 50 | |
| 8060090 | Stand use Path Grading | EA | 1285 | |
| 807050 | Natural Stone Markers | EA | 112 | |
| 8100371 | Post, Steel, 3/16 | EA | 44 | |
| 8100405 | Sign, Type III B | EA | 1 | |
| 8107050 | Roadside Sign Relocation, Special | EA | 1 | |
| 8117001 | Part Mrg, Only Cold Plastic, 24 inch, Special Emphasis Crosswalk | EA | 190 | |
| 8120012 | Barricade, Type III, High Intensity, Double Sided, Lighted, Furn | EA | 1 | |
| 8120013 | Barricade, Type III, High Intensity, Double Sided, Lighted, Oper | EA | 1 | |
| 8120140 | Lighted Arrow, Type C, Furn | EA | 2 | |
| 8120141 | Lighted Arrow, Type C, Oper | EA | 2 | |
| 8120170 | Minor Traf Devices | EA | 1 | |
| 8120261 | Plastic Drum, High Intensity, Lighted, Furn | EA | 75 | |
| 8120261 | Plastic Drum, High Intensity, Lighted, Oper | EA | 75 | |
| 8120350 | Sign, Type B, Temp, Prismatic, Furn | EA | 306 | |
| 8120351 | Sign, Type B, Temp, Prismatic, Oper | EA | 306 | |
| 8120370 | Traffic Regulator Control | EA | 1 | |
| 8137001 | Armor Stone | EA | 125 | |
| 8137001 | Quarried Rock Steps | EA | 75 | |
| 8137011 | Natural Stone Stabilization (8-12 inch) | Syd | 30 | |
| 8157050 | Bicycle Rack w/ Conc Pad | EA | 2 | |
| 8157050 | Permeable Paver Landing | EA | 1 | |
| 8157050 | Waste/Recycling Bceptacles w/ Conc Pad | EA | 2 | |
| 8157050 | American Beach Grass | EA | 6187 | |
| 8157050 | Berch w/ Conc Pad | EA | 2 | |
| 8157050 | Black Gum | EA | 5 | |
| 8157050 | Lanceloat Coreopsis | EA | 2063 | |
| 8157050 | Nodding Pink Onion | EA | 2063 | |
| 8157050 | Pink Pride Highberry | EA | 8 | |
| 8157050 | Sunburst Thomness Honeylocust | EA | 5 | |
| 8157050 | Irrigation System | EA | 1 | |
| 8160020 | Fertilizer, Chemical Nutrient, C/A | EA | 63 | |

| NO. | REVISION DESCRIPTION | BY | DATE |
|-----|----------------------|------|----------|
| 2 | REVISED SUBMITTAL | T.L. | 4-24-14 |
| 1 | Q1 SUBMITTAL | T.L. | 11-23-12 |

CITY OF TRAVERSE CITY
 ENGINEERING DEPARTMENT
 400 BOARDMAN AVE.
 TRAVERSE CITY, MI, 49684

DRAWING TITLE:
 WEST END BEACH TRAIL AND ACCESS PROJECT
 QUANTITY SHEET

SCALES:
 VERTICAL: N/A
 HORIZONTAL: N.T.S.
 DATE: DECEMBER 2011
 DESIGNED: T.L.
 DRAWN: JMK
 APPROVED: T.L.
 JOB NO.: 115841A

SHEET
11
 OF 11 SHEETS



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: JERED OTTENWESS, CITY MANAGER

SUBJECT: EMERGENCY REPAIRS AT OLD TOWN PARKING DECK
ELEVATOR

Attached is a memo from Nicole VanNess, Parking Administrator, regarding the emergency repair to the elevator controls at the Old Town Parking Deck for damage that occurred over the 2013/2014 winter season. The City has received reimbursement from MML in the amount of \$8,403.70, which is the amount after the \$5,000 deductible has been met.

I recommend the following motion (5 affirmative votes required):

that the competitive bidding process be waived; and that the City Manager be authorized to issue a confirming service order in the amount of \$13,403.70 to ThyssenKrupp Elevator for the emergency repair of elevator controls, with funds available in the Parking Fund.

JO/jd

cc: Nicole VanNess, Parking Administrator
Rob Bacigalupi, DDA Director

memo



Traverse City Parking Services

To: Jered Ottenwess, City Manager
From: Nicole VanNess, Parking Administrator
Date: July 30, 2014
Re: ThyssenKrupp Elevator Corporation Confirming PO Expenditure

Last winter, the amount of snow throughout the season caused damage to an elevator at the Old Town Parking Garage when it started to melt in the spring. Electrical components in the Northeast elevator tower were affected when a frozen drain began to thaw and the water backed up, penetrating the elevator roof. The damage occurred around March 17, 2014. ThyssenKrupp was called to evaluate the unit. They concluded the startup controller, and all electronic boards would need to be replaced along with a contaminated oil reservoir.

The repair costs were submitted as an insurance claim to the Michigan Municipal League. Our deductible was \$5,000, and MML issued a check for \$8,403.70 on April 3, 2014 which was deposited into 585-585-677.00 (reimbursements).

We have a three year service contract with ThyssenKrupp from March 2013-February 2016. At the time that the service was needed, it was considered an urgent matter. We did not receive competitive bids prior to moving forward with the work.

I would like to request the expenditure of \$13,403.70 for ThyssenKrupp Elevator Corporation from 585-585-930.00 (repairs and maintenance) to cover the cost of the repairs.



Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: CITY COMMISSION AD HOC COMMITTEE ON SIDEWALK CAFÉS

MAYOR PRO TEM JIM CARRUTHERS, CHAIRMAN
COMMISSIONER BARBARA BUDROS
COMMISSIONER JEANINE EASTERDAY

SUBJECT: ORDINANCE AMENDMENT - SIDEWALK CAFÉ

Attached is a previously submitted summary from the City Commission Ad Hoc Committee on Sidewalk Café's. Below are the recommendations as provided by the ad hoc committee.

To be effective *beginning April 1, 2015* (the next Sidewalk Café season):

- Requirement that a clear linear pedestrian pathway of at least five (5) feet in width be maintained at all times. This is an increase over the current requirement of a four (4) foot clear pedestrian pathway and was supported by Jim Moore, Executive Director for the Disability Action Network.
- That there be a default requirement that all café operators install a barrier to surround the sidewalk café that is anchored in the sidewalk (with the anchoring system to be approved by the City Engineer prior to its installation) – to ensure tables and chairs aren't moved, so that the 5'-wide clear pedestrian pathway is maintained. This requirement could be waived by Assistant City Manager Penny Hill, who serves as the ADA Coordinator, if it is determined the barrier is not necessary to ensure the clear pathway is maintained.
- That all furniture and equipment associated with the café be removed from city property when the café isn't in operation, i.e., at closing each day.

To be effective *10 days following enactment* of the ordinance:

- Permit holders shall be responsible for the proper disposal of all waste associated with their café occupancy and ensure it is removed on a daily basis.
- Prohibit the issuance of a permit for the following year for any business that has two (2) or more outdoor café ordinance violations in a year – such as a citation for not maintaining the minimum clear pedestrian pathway.

If the Commission enacts the ordinance, the City Clerk will send a letter to all city food service establishments informing them of the changes; and for the changes that would go in effect next permit season, this would give permit holders plenty of advance notice.

We recommend the motion:

that an amendment to the Traverse City Code of Ordinances, *Sidewalk Café Regulation Changes*, Sections 1020.07 and 1020.08, which make various changes to the regulations regarding sidewalk cafés (outdoor cafés), as recommended by the City Commission Ad Hoc Committee Regarding Sidewalk Cafés, which was introduced on July 21, 2014, be enacted with an effective date of August 14, 2014.

:BCM

e-copy: Russell Soyring, City Planning Director
Rob Bacigalupi, Downtown Development Authority Executive
Director
Dave Green, Director of Public Services
Timothy Lodge, City Engineer
Jim Moore, Disability Action Network

K:\tcclerk\city commission\ordinance amendments\sidewalk café ordinance enact

TRAVERSE CITY CODE OF ORDINANCES

ORDINANCE AMENDMENT NO. _____

Effective date: _____

TITLE: SIDEWALK CAFE REGULATION CHANGES

THE CITY OF TRAVERSE CITY ORDAINS:

That Sections 1020.07, *Sidewalk Café Permit*; and 1020.08, *Sidewalk Café with Alcohol Permit*, of the Traverse City Code of Ordinances, be enacted/amended to read in its entirety as follows:

1020.07 SIDEWALK CAFÉ PERMITS.

(a) Permit Conditions. The City Clerk may issue to a ~~an adjacent~~ food service establishment, a revocable sidewalk café permit to occupy a portion of adjacent City right-of-way to place tables and chairs, planters and windscreens ~~adjacent to the tables and chairs~~ in conjunction with selling and consuming food and beverages under the following terms and conditions:

(1) Prohibitions. The occupancy must not:

- A. Interfere with the use of the right-of-way for pedestrian or vehicular travel.
- B. Unreasonably interfere with the view, access to, or use of property adjacent to said street.
- C. Reduce the pedestrian travel area of any sidewalk to less than four (4) feet in width; a clear linear path at least four (4) feet in width must be maintained at all times; and effective April 1, 2015, not reduce the pedestrian travel area of any sidewalk to less than five (5) feet in width; a clear linear path at least five (5) feet in width shall be maintained at all times. The pedestrian travel area shall not include trees, bushes, walls, parking meters, fire hydrants, tree grates or any other fixtures permanently located in the right-of-way.
- D. Cause damage to the street or sidewalk or to trees, benches, landscaping, or other objects lawfully located in the right-of-way. ~~Any~~ The sidewalk anchoring system to secure an item in ~~to~~ the sidewalk shall be approved by the City Engineer ~~Manager~~ in writing prior to installation.
- E. Cause a violation of any federal, state or local laws.
- F. Be ~~principally~~ used for off-premises advertising. All signs must conform to the Sign Ordinance.
- G. Conceal or detract from the appearance of landscaping features in or adjacent to the street.
- H. Be in or adjacent to property zoned exclusively for residential purposes.

- I. Be attached to or reduce the effectiveness of or access to any utility pole, sign or other traffic control device.
 - J. Cause increased risk of theft or vandalism.
 - K. Violate regulations adopted by the City Manager pursuant to this Code.
 - L. Serve alcohol unless the business holds an Sidewalk Café with Alcohol Permit issued by the City Clerk.
 - M. Leave any furniture or equipment associated with the Sidewalk Café on public property when the café is not in operation. This subsection shall be effective April 1, 2015.
 - N. May not operate beyond 11 p.m.
- (2) Notice. Notice to the adjacent property owners or occupants on both sides of the applicant's property shall be required before issuing a permit to occupy any right-of-way area between the edge of the vehicle use area of the right-of-way and the right-of-way property line. Said notice should include an enumeration of the conditions or rights provided under Section 1020.07(1) of this Code.
- (3) Fee. Prior to the issuance of a permit, a fee in an amount established by resolution of the City Commission shall be paid to the City Clerk. This fee shall be tripled if any such occupancy occurred prior to the issuance of a permit.
- (4) Insurance. The Permittee shall show proof of and maintain comprehensive general liability insurance and have the City as an additional named insured. The amount of such insurance shall be determined by the City Clerk.
- (5) Waste. Any holder of a Sidewalk Café Permit shall be responsible for the proper disposal of all waste associated with their Sidewalk Café occupancy and shall ensure that the waste is removed on a daily basis.
- (6) Food Service Establishment. Food service establishment shall be defined in accordance with its meaning in the Food Law of 2000, as amended. MCLA 289.1101.
- (7) Regulations. The City Manager may adopt an executive order controlling the occupancy pursuant to a Sidewalk Café Permit.
- (8) Site Plan and Barrier Requirement. A site plan shall be submitted with the application showing where a barrier will be placed to maintain an unobstructed foot path five (5) feet wide for pedestrians at all times, to ensure compliance with the Americans with Disabilities Act. A barrier shall delineate the sidewalk café and generally be 36" in height and be approved by the City Planning Director. The placement of any required barriers shall be marked on the sidewalk by the Code Enforcement Officer and shall include the sidewalk anchoring system approved by the City Engineer; the barrier shall be such that it is sufficient to ensure compliance with the approved site plan. The barrier shall be removed when the business closes each day. If the city's ADA Coordinator determines that existing conditions are such that a barrier is not required to maintain an unobstructed foot access path at least five (5) feet wide for pedestrians, the requirement for the barrier may be waived by the city's ADA Coordinator. This subsection shall be effective April 1, 2015.
- (9) Compliance with Approved Site Plan. The café shall, at all times, comply with the approved site plan.

- (b) Duration. Permits shall be for the period of April 1 to November 1 for the year in which granted.
- (c) Display. A permit shall only be valid if displayed in a manner visible to the public.
- (d) Permit Revocation. Any permit may be revoked by the City Clerk upon a finding that the occupancy does not meet the standards of this Code, any other provisions of this Code, or other applicable law or regulation, or that the right-of-way is needed for other street or utility purposes. Upon such revocation, the fee paid for any period after termination of the street occupancy shall be refunded and the right-of-way shall be restored at the expense of the Permittee.
- (e) Appeal. Persons who are refused a permit or have had their permit revoked may request in writing a hearing on that determination before the City Manager. The decision of the Manager may be appealed to the City Commission. Requests for a hearing or an appeal must be made within five (5) days of the questioned decision.
- (f) Non-renewal of permit. If a permit holder has two (2) or more violations in a permit year, the City Clerk shall not renew the permit the following permit year.
- (g) Appearance Tickets. The Police Chief and the appointed officers of the Police Department, or such other officials as are designated by the City Manager, are hereby authorized to issue and serve appearance tickets with respect to a violation of this chapter pursuant to Section 1 of Act 147 of the Public Acts of 1968, as amended (MCLA 764.9c(2); MSA 28.868(3)(2)). Appearance tickets shall be in such form as determined by the City Attorney and shall be in conformity with all statutory requirements.
- (h) Violations. A person who violates this section is responsible for a civil infraction. (Ord. 368. Passed 8-16-93. Ord. 539. Passed 7-26-01. Ord. 570. Passed 7-1-02. Ord. 658. Passed 12-6-04 Ord. 700. Passed 4-3-06)

1020.08 SIDEWALK CAFÉ WITH ALCOHOL PERMIT.

- (a) Permit Conditions. The City Clerk may issue to ~~an adjacent~~ a food service establishment a revocable Sidewalk Café with Alcohol Permit to occupy a portion of adjacent City right-of-way to place tables and chairs, planters and windscreens ~~adjacent to the tables and chairs~~ in conjunction with selling and consuming food and alcoholic beverages, provided the following terms and conditions are met:
 - (1) May not operate beyond 11 p.m.
 - (2) Service of alcohol at the Sidewalk Café does not violate any state, federal or local laws, promulgated rules, or policies or executive orders of the City Manager.
 - (3) A barrier shall surround the sidewalk café; it shall be approved by the City Planning Director, generally be 36" in height, and shall be removed when the establishment closes each day.
 - (4) A site plan shall be submitted with the application showing where a barrier will be placed to maintain an unobstructed foot path five (5) feet wide for pedestrians at all times, to ensure compliance with the Americans with Disabilities Act. The placement of any required barriers shall be marked on the sidewalk by the Code Enforcement Officer and shall include the sidewalk anchoring system approved by the City Engineer; the barrier shall be such that it is sufficient to ensure compliance with the approved site plan. The barrier shall be removed when the business closes each day. If the city's ADA Coordinator determines that existing conditions are such that a barrier is not required to maintain an unobstructed foot

access path at least five feet wide for pedestrians, the requirement for the barrier may be waived by the city's ADA Coordinator. This subsection shall be effective April 1, 2015.

- (5) The business must post a sign in a prominent location that is one (1) square foot that indicates, "No beverages beyond the barrier of this Sidewalk Café." Specifically, the sign shall be posted within the perimeter of the Sidewalk Café.
- ~~(6) Alcohol may not be served beyond 11 p.m.~~
- (7) Prohibitions. The occupancy must not:
- A. Interfere with the use of the right-of-way for pedestrian or vehicular travel.
 - B. Unreasonably interfere with the view, access to, or use of property adjacent to said street.
 - C. Reduce the pedestrian travel area of any sidewalk to less than four (4) feet in width; a clear linear path at least four (4) feet in width must be maintained at all times. Effective April 1, 2015, the occupancy must not reduce the pedestrian travel area of any sidewalk to less than five (5) feet in width; a clear linear path at least five (5) feet in width must be maintained at all times. The pedestrian travel area shall not include trees, bushes, walls, parking meters, fire hydrants, tree grates or any other fixtures permanently located in the right-of-way.
 - D. Cause damage to the street or sidewalk or to trees, benches, landscaping, or other objects lawfully located in the right-of-way. ~~Any~~ The sidewalk anchoring system to secure an item ~~in~~ to the sidewalk shall be approved by the City ~~Engineer~~ Manager in writing prior to installation.
 - E. Cause a violation of any state or local laws.
 - F. Be ~~principally~~ used for off-premises advertising. All signs must conform to the Sign Ordinance.
 - G. Conceal or detract from the appearance of landscaping features in or adjacent to the street.
 - H. Be in or adjacent to property zoned exclusively for residential purposes.
 - I. Be attached to or reduce the effectiveness of or access to any utility pole, sign or other traffic control device.
 - J. Cause increased risk of theft or vandalism.
 - K. Leave any furniture or equipment associated with the Sidewalk Café on public property when the café is not in operation. This subsection shall be effective April 1, 2015.
 - L. Violate regulations adopted by the City Manager pursuant to this Code.
- (8) Notice. Notice to the adjacent property owners or occupants on both sides of the applicant's property shall be required before issuing a permit to occupy any right-of-way area between the edge of the vehicle use area of the right-of-way and the right-of-way property line. Said notice should include an enumeration of the conditions or rights provided under Section 1020.08(1) of this Code.

- (9) Fee. Prior to the issuance of a permit, a fee in an amount established by resolution of the City Commission shall be paid to the City Clerk. This fee shall be tripled if any such occupancy occurred prior to the issuance of a permit.
- (10) Insurance. The Permittee shall show proof of and maintain comprehensive general liability insurance and liquor liability insurance and have the City of Traverse City as an additional named insured. The amount of such insurance shall be determined by the City Clerk.
- (11) Waste. Any holder of an Sidewalk Café with Alcohol Permit shall be responsible for the proper disposal of all waste with their Sidewalk Café occupancy and shall ensure that the waste is removed on a daily basis.
- (12) Compliance with Approved Site Plan. The café shall, at all times, comply with the approved site plan.
- (13) Food Service Establishment. Food service establishment shall be defined in accordance with its meaning in the Food Law of 2000, as amended. MCLA 289.1101.
- (14) Regulations. The City Manager may adopt an executive order controlling the occupancy pursuant to a Sidewalk Café with Alcohol Permit.
- (15) Agreement For Cafes not Directly Adjacent to Building Required. For Sidewalk Cafes where the café area is on sidewalk that is adjacent to the building but configured so that the pedestrian walkway is between the building and the café area, the owner of the Sidewalk Café shall enter into an Sidewalk Café License Agreement with the city further specifying the terms and conditions of the permit. The City Clerk is authorized to execute such agreements on behalf of the city.
- (16) Duration. Permits shall be for the period of April 1 to November 1 for the year in which granted.
- (17) Display. A permit shall only be valid if displayed in a manner visible to the public.
- (18) Permit Revocation. Any permit or License Agreement may be revoked by the City Clerk upon a finding that the occupancy does not meet the standards of this Code, any other provisions of this Code, or other applicable law or regulation, or that the right-of-way is needed for other street or utility purposes. Upon such revocation, the fee paid for any period after termination of the street occupancy shall be refunded and the right-of-way shall be restored at the expense of the Permittee.
- (19) Appeal. Persons who are refused a permit or have had their permit revoked may request in writing a hearing on that determination before the City Manager. The decision of the Manager may be appealed to the City Commission. Requests for a hearing or an appeal must be made within five (5) days of the questioned decision.
- (20) Non-renewal of permit. If a permit holder has two (2) or more violations in a permit year, the City Clerk shall not renew the permit the following permit year.
- (21) Appearance Tickets. The Police Chief and the appointed officers of the Police Department, or such other officials as are designated by the City Manager, are hereby authorized to issue and serve appearance tickets with respect to a violation of this chapter pursuant to Section 1 of Act 147 of the Public Acts of 1968, as amended (MCLA 764.9c(2); MSA 28.868(3)(2)). Appearance tickets shall be in

such form as determined by the City Attorney and shall be in conformity with all statutory requirements.

(22) Violations. A person who violates this section is responsible for a misdemeanor.

(Ord. 700. Passed 4-3-06)

The effective date of this Ordinance is the _____ day of _____, 2014.

I hereby certify the above ordinance amendment was introduced on _____, at a regular meeting of the City Commission and was enacted on _____, at a regular meeting of the City Commission by a vote of Yes: ____ No: ____ at the Commission Chambers, Governmental Center, 400 Boardman Avenue, Traverse City, Michigan.

Michael Estes, Mayor

Benjamin C. Marentette, City Clerk

I hereby certify that a notice of adoption of the above ordinance was published in the Traverse City Record Eagle, a daily newspaper published in Traverse City, Michigan, on _____.

Benjamin C. Marentette, City Clerk



Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ^{JO} JERED OTTENWESS, CITY MANAGER

SUBJECT: TRAVERSE CITY PUBLIC PIER STUDY GROUP CONSULTANT
RECOMMENDATION

Attached is a memo from City Planning Director Russell Soyring indicating the Traverse City Public Pier Study Group's recommendation that SmithGroup JJR be selected as the consultant for the Traverse City Public Pier Project. Another memo is attached from Mr. Soyring which indicates, based on the Group's recommendation, staff's recommendation that a contract be authorized with SmithGroup JJR in the amount of \$232,000 for the work.. The entire cost of the consultant's work will be covered by a \$232,000 Great Lakes Fishery Trust Grant.

On Monday evening, Planning and Engineering Assistant Missy Luick, who has served as the Project Liaison, will provide a brief overview; and then Commissioner Ross Richardson, who has served on the Study Group, will offer some comments. Mayor Michael Estes also served on the Study Group as a representative of the City Commission.

The work to be provided under the agreement includes:

- Field investigation
- Shoreline studies
- Public outreach
- Design development
- Preliminary engineering
- Permitting and construction drawings

The following summarizes Commission actions regarding this project:

- 1.) May 20, 2013 – Resolution Supporting \$5,000 grant application to Rotary Charities of Traverse City for initial study of environmental impacts of this project.
- 2.) July 1, 2013 – Resolution Supporting Grant Application to Great Lakes Fishery Trust for final design, engineering, environmental studies and related work to construction the public pier
- 3.) February 4, 2014 – Resolution Accepting \$232,000 grant from the Great Lakes Fishery Trust

I recommend the following motion (5 affirmative votes required):

that the Mayor and City Clerk execute an agreement with SmithGroup JJR in the amount of \$232,000 for services associated with the Traverse City Public Pier, including field investigation, shorelines studies, public outreach, design development, preliminary engineering, and permitting and construction drawings, with the contract cost to be fully paid by grant funds received from the Great Lakes Fishery Trust from the Capital Projects Fund; and furthermore that a budget amendment be made to increase both budgeted revenue and budgeted expenses by \$232,000 to reflect the receipt of the grant and the expenditures thereof.

JJO/bcm

K:\tcclerk\city commission\agreements\public pier consultant

copy: Russell Soyring, City Planning Director
Missy Luick, Planning and Engineering Assistant – Traverse City Pier
Project Liaison

Memorandum

The City of Traverse City
Planning Department



TO: Jered Ottenwess, City Manager

FROM: Russell A. Soyring, Planning Director 

DATE: July 23, 2014

SUBJECT: Traverse City Public Pier Study Group Consultant Recommendation

A staff led Traverse City Public Pier Study Group was assembled in March 2014 to do the following:

1. To finalize and disseminate a request for qualifications (RFQ) to perform the scope of work in the Great Lakes Fishery Trust grant award.
2. To screen and interview candidates who respond to the RFQ.
3. To recommend a candidate to the City of Traverse City, recognizing that the City Commission will make ultimate decisions involving the project consultant.
4. To help engage all interests in the process.
5. To stay informed of project status and steps and provide input through the design phase of the project.

The Traverse City Public Pier Study Group members are:

- Michael Estes, City of Traverse City Mayor
- Ross Richardson, City Commissioner
- Cathie Ballard, City Resident and former director of the Michigan Coastal Zone Management Program
- Todd Kalish, Michigan DNR Fisheries Division
- Jack Knol, Architect
- Steve Largent, Grand Traverse County Conservation District
- Ryan Matuzak, Grand Traverse Sport Fishing Association
- Erik Olsen, Grand Traverse Band of Ottawa and Chippewa Indians and Member, Scientific Advisory Team to the Great Lakes Fisheries Trust Board of Trustees

The Pier Study Group met on April 10, April 29, June 24, July 9 and July 18. A Traverse City Public Pier project page was created on the City's website: <http://www.traversecitymi.gov/tcpubicpier.asp> Project information, meeting postings, RFQ responses, etc. have been uploaded to the project page.

The Traverse City Public Pier RFQ was released on May 15, 2014 and responses were due on June 12, 2014. The City received seven responses to the RFQ:

- Beckett and Raeder (Ann Arbor, MI)

- Fleis and VandenBrink (Traverse City, MI)
- Hubbell, Roth & Clark, Inc. (Bloomfield Hills, MI)
- PORT Architecture + Urbanism, LLC (Chicago, IL)
- SmithGroup JJR (Ann Arbor, MI)
- Spicer Group, Inc. (Saginaw, MI)
- URS Corporation (Traverse City, MI)

The Pier Study Group evaluated each response by using a Pier RFQ Rating Form. By consensus, the Study Group eliminated the three lowest rated firms from consideration. They were: URS, Hubbell Roth and Clark and Port Architecture. The study group discussed whether they should interview 3 or 4 firms. After some discussion, they decided to interview 4 firms: SmithGroupJJR, Beckett and Raeder, Spicer Group and Fleis and Vandenbrink.

The Pier Study Group conducted interviews on July 9. Overall, the group was impressed with all the four firms that interviewed. The group decided to eliminate Spicer Group and Beckett and Raeder after some deliberation.

After further discussion, it was decided that the Group would like to solicit an additional written response from SmithGroupJJR and Fleis and Vandenbrink of the following questions:

1. Durability/materials selection process and maintenance
2. Breakout of public involvement hours
3. Explain ideas for connectivity/access to the pier
4. More information about coastal/marine experience

The Additional Information Request was due on July 16. The Study Group met again on July 18 to consider the additional information and make a recommendation to the City Commission.

Following discussion by the Group, the following motion was made at the July 18, 2014 Traverse City Public Pier Study Group meeting:

Motion by Commissioner Richardson, second by Mr. Largent, to recommend SmithGroupJJR as the consultant for the Traverse City Public Pier project to the City Commission.

Motion carried 4-2. (Mayor Estes, Commissioner Richardson, Mr. Largent, Mr. Matuzak in support; Mrs. Ballard and Mr. Kalish opposed; Mr. Knol and Mr. Olsen absent)

Attachments: SmithGroupJJR RFQ response, SmithGroupJJR Hours and Tasks handout, SmithGroupJJR Additional Information Response

RAS:mll

Memorandum

The City of Traverse City
Planning Department



TO: Jered Ottenwess, City Manager

FROM: Russell A. Soyring, Planning Director 

DATE: July 30, 2014

SUBJECT: Traverse City Public Pier- Consultant Agreement Recommendation

Following the recommendation by the Traverse City Public Pier Study Group to hire SmithGroupJJR as the consultant for the Traverse City Public Pier project, staff recommends that the proper City officials be authorized to execute a Consultant Agreement with SmithGroupJJR in the amount of \$232,000 for completing the field investigations, shoreline studies, public outreach, design development, preliminary engineering, permitting, and construction drawings and details for the Traverse City Public Pier project, with funds available through a Great Lakes Fisheries Trust grant accepted by the City Commission for this project.

RAS:mll

Innovative. Waterfront. Design.

Request for Qualifications
Traverse City Public Pier

June 12, 2014

SMITHGROUP JJR

salmon

laker

steelhead

walleye

smallmouth

imagine a pier.



imagine access

E B A Y

W A Y

R D M A N R I V E R



June 12, 2014

Ms. Missy Luick
Planning and Engineering Assistant
City of Traverse City
2nd Floor, Governmental Center
400 Boardman Avenue
Traverse City, Michigan 49684

RE: SmithGroupJJR Response to Request for Qualifications: Traverse City Public Pier

Dear Ms. Luick:

SmithGroupJJR is pleased to provide our response to the City's Request for Qualifications (RFQ) regarding the Traverse City Public Pier planned at the mouth of the Boardman River in the west bay of Grand Traverse Bay. First off, congratulations to the City for securing a Fisheries Trust Fund Grant for this project; this grant signifies that your project is considered among the best proposed for providing public access to the waterfront, and allowing passive and active recreation to the waters of the Great Lakes State!

Traverse City should also be recognized for its commitment to establishing a waterfront master plan and working toward its implementation. The *Your Bay, Your Say* initiative, which continued through the West Bayfront Master Plan, is an excellent road map. It will help position Traverse City as a top contender for attracting and retaining businesses, residents, visitors and conferences and being listed in the top tier of places to live and visit in Michigan, the region and nationally.

SmithGroupJJR is proud of our involvement with the City's Bayfront Master Plan, as well as our work on the Clinch Marina. We have a long-term, proven relationship with the City and continue to monitor the master plan implementation progress with excitement and interest. Although our team continues to visit and observe the variety of bay front improvements, this does not differentiate our firm or make us uniquely qualified for this next opportunity—everybody loves to visit Traverse City and many other firms have also worked with the City over the years.

What differentiates SmithGroupJJR, and what uniquely qualifies our team to collaborate with the City on this project is:

Passion – Our team is passionate about waterfronts, passionate about creating special places and passionate about placing communities in Michigan on the map for places to live, work and play.

Community Knowledge – We have a familiarity with Traverse City through our public engagement efforts with the Bayfront Master Plan, and the continued interaction with the City and region. Our team is supplemented by a local engineering firm, Gourdie-Fraser Associates, a firm with roots in the City and knowledge of its systems.

Green Infrastructure Expertise – We are leaders in sustainable, community-based planning that makes cities more walkable, connected and attractive to a diverse population. Green infrastructure is more than just stormwater management and greenways; it's about connectivity, placemaking, asset and value management and understanding of a community's special needs.

National Waterfront Expertise – From Navy Pier in Chicago to Elmwood Township’s Waterfront District Master Plan, we have built much of our practice on providing innovative planning and design for waterfront communities across North America. Specifically, no other firm has played a greater role in the design and planning of the great waterfronts of the Great Lakes. We understand the water, its assets and the technical issues associated with littoral drift, coastal wave environments, the natural systems of coastal zone wetlands and the forces that coastal and waterfront structures are subject to during construction and throughout the life of the structures and facilities.

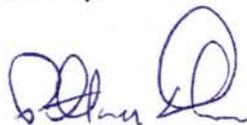
Project Leadership, Management and Facilitation – We are known for project and client leadership; management of process facilitation; cost understanding and scheduling; facilitation; and development of implementation strategy. Our project management, based on communication, quality and knowledge, has resulted in a comprehensive portfolio of successful projects from vision to implementation and beyond, including operation, maintenance and warrantee management.

Innovation – In every aspect of our work, we encourage innovation—from our team and partners to clients, owners, the public, users and contractors. Innovation of process, materials, systems, approaches and strategies improve the quality, delivery and use of spaces and is critical to the creation of special and unique community environments. It is critical and free thinking that creates our passion for public spaces—the freedom to think outside of the ordinary and explore what makes a community successful.

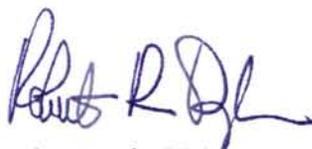
Thank you, again, for the opportunity to respond to the City’s latest bay front implementation initiative. SmithGroupJJR has a sincere desire to continue our project experience and working relationship with Traverse City. We are eager about the possibility of interviewing to discuss our capabilities in greater depth. In the interim, if you have questions or require any additional information, please contact Pat Doher, team leader and direct contact for this project, at 734.669.2766 or pat.doher@smithgroupjjr.com

Thank you for the opportunity, and congratulations again on winning the grant!

Sincerely,



Patrick M. Doher, PE, LEED AP
Senior Vice President



Robert Doyle, RLA
Principal

PRIMARY CONTACT:

Pat Doher, Senior Vice President

201 Depot Street, Second Floor
Ann Arbor, Michigan 48104
pat.doher@smithgroupjjr.com
734.669.2766

OUR TEAM/

SMITHGROUP JJR

SmithGroupJJR is the longest continuously operating architecture, engineering, and planning firm in the U.S. With a staff of over 800 in 9 regional offices throughout the country and 1 in Shanghai, SmithGroupJJR's integrated practice offers depth in all disciplines serving the built and natural environment, including architecture, engineering, landscape architecture, urban design, and environmental science.

Depth of Waterfront Experience.

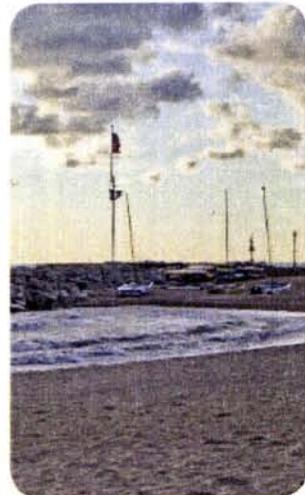
As a national leader in waterfront design, SmithGroupJJR's highly experienced waterfront designers, coastal engineers, and urban planners have provided innovative planning and design, coupled with the most current technical approaches and standards, for waterfront communities across North America. We have a proven track record of helping clients get the most out of their waterfront investments.

Sustainable Design.

Planning and design initiatives should build resilient economies and communities, regenerate damaged ecosystems, encourage healthier lifestyles, and reduce global impacts. We recognize that urban planning issues are highly interrelated across scales and systems and that integrating sustainability principles throughout the planning process allows us to respond efficiently to complex issues and develop solutions that enrich the urban experience, breathing new life, social equity, and economic vitality into the community.

Permitting + Regulatory Coordination.

SmithGroupJJR's multidisciplinary team can accurately identify the federal, state, and local permitting requirements of your project and our experience with and knowledge of local regulatory agencies facilitates interactions that represent the client's best interests.



Gourdie-Fraser, Inc.

GFA, established in downtown Traverse City in 1948 as the John C. Norton Company, is celebrating its 66th year in the City of Traverse City. We've completed well over 100,000 projects in that time, including many notable landmark projects within the city limits and studies that weigh options, like Boardman Lake Avenue. As long-term residents of the city we are proud to contribute to the city's success and vitality, and the region's economic strength as a whole.



Interpretive Ideas
Planning Training Design GIS

Interpretive Ideas develops plans and educational materials for natural resource agencies, parks, waterfronts, and recreation destinations. With a focus on integrating signage into the concept of a site, projects include interpretive plans, sign and exhibit design, training workshops, college courses, and educational publications. Interpretive Ideas has worked with various notable clients, including the Michigan Department of Natural Resources, U.S. Army Corps of Engineers, The Nature Conservancy, the Port Huron Community Foundation, the USDA Forest Service, and several other municipal agencies and architectural firms.

TEAM MEMBERS /

Patrick M. Doher, PE, LEED AP

Principal-in-Charge | SmithGroupJJR

Patrick has extensive experience managing urban design, waterfront, and civil engineering projects that involve river and lakefront recreational facilities, parks, and public and private developments. He successfully works with clients from the concept phase, through permitting, to implementation and completion of construction.



Education

BS, Civil Engineering,
Wayne State University,
1981

Registrations

Professional Engineer:
FL, IL, MI, OH, OR

Elmwood Marina District Plan,
Elmwood Township, Michigan

Navy Pier Marina Feasibility Study
and Revitalization, Chicago, Illinois

Bay County Waterfront
Development Study, Bay County,
Michigan

William G. Milliken State Park and
Harbor, Detroit, Michigan

South Haven Marina Park and
Riverfront Improvements Plan,
South Haven, MI

Robert Doyle, ASLA

Project Manager | SmithGroupJJR

With over 27 years of professional experience, Bob successfully serves public, institutional and private sector clients as a project manager and landscape architect. The project types managed and designed by Bob include parks and recreation, campus planning and improvements, community planning and urban design, brownfield redevelopment and waterfronts.



Education

B, Landscape Arch.,
Michigan State
University, 1985

Registrations

Landscape Architect: MI
Residential Builder: MI

Elmwood Marina District Plan,
Elmwood Township, Michigan

Fishtown Site Study Design and
Master Plan, Leland, Michigan

Kalamazoo Harbor Master Plan,
Saugatuck and Douglas, Michigan

Sheboygan Marina and Public
Launch Facility, Sheboygan,
Wisconsin

William G. Milliken State Park and
Harbor, Marina Enhancements,
Detroit, Michigan

Neal Biletdeaux, ASLA, LEED AP BD+C

Environmental Designer + NEPA Specialist | SmithGroupJJR

Neal's strong background in ecology gives him a unique understanding of how environmental issues play an important role in today's planning projects. He has worked on projects ranging from habitat restoration of terrestrial and aquatic ecosystems to site planning and design at multiple scales with emphasis on ecosystem and stormwater management.



Education

MLA, Landscape
Architecture, University
of Michigan, 1987

Registrations

Landscape Architect: MI

Blue Water Bridge Landscape/
Wetland Enhancement, Port Huron,
Michigan

Portage Lakefront Park and
RiverWalk, Indiana Dunes National
Lakeshore, Portage, Indiana

Glen Haven Village Historic District,
Sleeping Bear Dunes National
Lakeshore, Glen Haven, Michigan

Boardman River Crossing Mobility
Study, Traverse City, Michigan

Detroit East RiverWalk, Detroit,
Michigan

Emily McKinnon, PE, LEED AP

Civil Engineer | SmithGroupJJR

Emily has 13 years of experience in design and engineering services for municipal, institutional, and commercial clients. She uses her expertise in stormwater management design, hydrologic/hydraulic analyses, utility design, grading, permit acquisition, and construction administration on a variety of cities and communities, parks and recreation, and waterfronts projects.



Education

BS, Engineering,
University of Michigan,
2000

Registrations

Professional Engineer:
MI, MD, OH, UT

Port Huron South Waterfront
Master Plan, Port Huron, Michigan

Shades Beach Park, Harborcreek
Township, Pennsylvania

Waterfront Master Plan and Beach
Expansion Sediment Transport
Study, Sister Bay, Wisconsin

Lower Rouge River Non-Motorized
Trail, Canton, Michigan

Huntington Reservation Shoreline
Management and Facilities Plan,
Bay Village, Ohio

TEAM MEMBERS /

Margaret Boshek, PE

Coastal + Marine Engineer | SmithGroupJJR

Margaret has over 9 years of experience in the ocean and coastal engineering field specializing in coastal structure design, waterfront development, and marina master planning and design. She has extensive experience with several technical modeling programs and field work experience including underwater survey and systems deployment.



Euclid Harbor Feasibility Study,
Euclid, Ohio

Fisherman's Cove Working
Waterfront, Bellingham, Washington

Department of Fisheries and Oceans
Condition Assessment, Batchawana
Bay, Ontario*

Marina Bay Marina Rubble Mound
Breakwater Rehabilitation, Quincy,
Massachusetts*

Education

MS, Coastal & Marine
Engineering and Manage-
ment, Delft University of
Technology, 2009

Registrations

Professional Engineer: BC

Ft. Pierce Marina Physical Model
Testing, Fort Pierce, Florida*

Keenan Gibbons, LEED Green Associate

Site Designer | SmithGroupJJR

Keenan's expertise includes urban design and planning, systems analysis, stormwater management, 3D modeling and construction documentation, architectural graphics and renderings, and digital media. As a Traverse City native, Keenan grew up spending time along the lake shore and on the Boardman River and has a unique familiarity with the project site.



Middlegrounds Metropark Master
Plan and Implementation, Toledo,
Ohio

Midtown Loop Greenway, Detroit,
Michigan

Mohawk River Pedestrian Bridge,
Amsterdam, New York*

City of Glens Falls Connectivity
Feasibility Study, Glens Falls, New
York*

Education

MLA, Landscape
Architecture, Ball State
University, 2011

Awards

2011 Indiana ASLA
Student Design Award

Daniel Wagner, PE

Civil Engineer | Gourdie-Fraser, Inc.



Education

BS, Environmental
Engineering, Mich-
igan Technological
University

Daniel has diverse experience by providing engineering and management services for various types of projects. His varied consulting experience has resulted in a thorough understanding of local, county, state and federal agency permitting and compliance requirements relative to these projects.

Munson Medical Center Tributary A
Relocation, Traverse City, Michigan

Peshawbestown Marina Design &
Permitting, Grand Traverse Band, Michigan

8th Street Streetscape and Boardman
River, Traverse City, Michigan

Gary Wilson, PS

Professional Surveyor | Gourdie-Fraser, Inc.



Education

Associate Degree,
Surveying, Ferris State
College

As a vice-president at Gourdie-Fraser, Gary has over 44 years of experience managing, mapping, and surveying various projects, including. His projects include bathymetric/hydrographic surveying, utility staking and mapping, boundary surveys, topographic mapping and more for various types of clients.

Manistee River Multibeam and Side Scan
Survey, Michigan

Shoal Monitoring - Various Locations along
Lake Michigan Shoreline, Michigan

Eastern Lake. Michigan Hydrographic
Mapping, Detroit District, Michigan

Lise Schools, CIP

Interpretive Signage | Interpretive Ideas



Education

MS, Education,
Northern Illinois
University
BS, Recreation,
Indiana University

Prior to founding Interpretive Ideas in 1993, owner Lise Schools was a field interpreter and nature center director. Her practical experience gives her an insight into visitor perceptions, staff needs related to interpretation, and implementing successful, integrated, and educational interpretive signage approaches.

Bluewater Riverwalk Interpretive Sign
Design, St. Clair County, Michigan

Milliken State Park Interpretive Design,
Detroit, Michigan

Reeds Lake Trail Signs, East Grand
Rapids, Michigan

DEMONSTRATION OF QUALIFICATIONS /

PRIOR EXPERIENCE

Prior experience constructing a pier in a Great Lake-type environment is a prequalification requirement.

SmithGroupJJR has had extensive experience working with Great Lakes communities across the Midwest. Much of our practice has been built on providing innovative planning and design for waterfront communities. Our highly experienced waterfront designers, coastal engineers, and urban planners provide the latest and best design approaches and technical standards for your project. In the following pages we showcase our experience relative to this project and believe you will see our knowledge and experience to be in line with the needs of your project.

Our Great Lakes Experience /

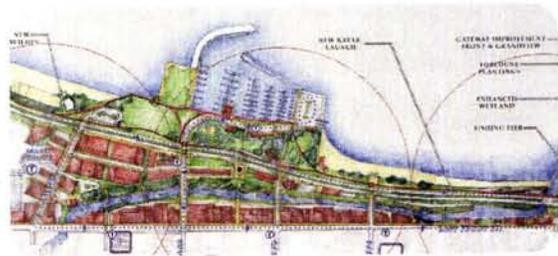


PUBLIC ENGAGEMENT

Using effective strategies and methods to meaningfully engage the general public, including project stakeholders to obtain input in completing similar projects.

A decision-making process that includes multiple interests is essential to successfully meeting planning and design challenges within waterfront communities. The effective communication of ideas and public-based collaborative planning and design processes have remained a hallmark of our work, and our staff possess exceptional skill at working in the public forum. We provide meaningful opportunities for key stakeholders, regulatory and resource agencies, and the public to collaborate in developing, evaluating and implementing project solutions.

We use a wide range of active involvement and communication techniques, including multi-media applications, to build project support and facilitate project implementation. Information is conveyed through newsletters, pamphlets, handouts, mass mailings, telephone hotlines, and other mass media communications. More active involvement includes on-site workshops, visioning, stakeholder outreach, visual listening exercises, citizen advisory groups, community and neighborhood workshops, partnering, advisory boards, and public hearings. These techniques help minimize and resolve project disputes and turn ideas into action.



"Your Bay Your Say"



Public Engagement Session

Responding to questions, ideas and concerns expressed by the general public with regard to similar projects.

Our goal is to effectively convey project information and provide hands-on opportunities for individuals to become actively involved in the decision-making process. We find creative solutions that satisfy and often blend differing points of view.

Effective management of public questions begins with listening carefully, and engaging the public to clarify the concerns behind the question. While

some inquiries from the public are not relevant to the particular project, in nearly every question, idea, or concern expressed there is a kernel of truth which must be addressed in a transparent and respectful way. We have worked on great number of projects which have generated both positive and negative response from a community, and our approach of treating all with respect and without condescension, has been key to our success in getting projects built.

BEST PRACTICES

Identifying and using best practices to achieve successful outcomes with similar projects.

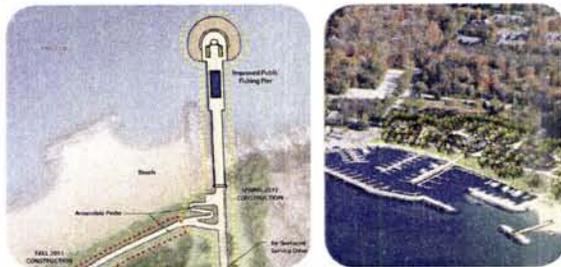
SmithGroupJJR has successfully planned, designed, and implemented a significant number of community based waterfront projects over the last three decades, and we are considered leaders in the Great Lakes region. This depth of experience has given us insights into the best practices for developing waterfront communities and projects, from the broad scale of community planning to the detail of creating waterfront fisheries habitats. Some of the best practices we advocate include:

Public Involvement

- Engage the community through a multi-level community outreach strategy
- Establish a set of goals for the project and for the community upfront, with input from residents, business people and stakeholder organizations
- Communicate planning and design ideas clearly, and provide the public with options to consider, and opportunities to participate in the planning

Balance Goals

- Create habitat while protecting and improving shorelines - it's the less expensive, desired by the community, and benefits the Great Lakes
- Build strong public/private partnerships and recognize common goals



Establish Connections

- Encourage the connection from the waterfront to the community core and amenities
- Create a place that attracts visitors, but also appeals to and serves residents
- Improve quality of life, which translates to a more economically vibrant community

Environmental Protection

- Inventory the environmental assets
- Encourage low impact design through practice and policy
- Protect important wetlands, fisheries habitat, and stream corridors
- Guide development in compact, coordinated patterns
- Link local actions to water quality and health of the valued resources

PERMITTING + STUDIES

Identification of all applicable permits that will or may be required by this project and all studies and construction documents shall be developed to meet all such anticipated permit requirements.

Many of our waterfront and marina projects require coordination with various state agencies in Michigan. SmithGroupJJR has demonstrated experience in the successful planning and execution of projects in full accordance with all applicable local, state, and Federal regulations and with efficient interaction with State of Michigan client agencies, including the MDNR Waterways Commission, MDEQ, and the Department of Technology Management and Budget Design and Construction Division. Our team of professionals has also worked on permit applications with the US Army Corps of Engineers, the Department of Natural Resources (DNR), the Environmental Protection Agency, the National Environmental Protection Act (NEPA) on Environmental Assessments and Environmental Impact Statements.

Our multidisciplinary team can determine the federal, state and local permitting requirements of your project and work quickly to prepare the required reports, studies, data, applications and other documentation required for efficient permit approval. In conjunction with the preparation of preliminary design development plans, our team works with the client and regulatory agency staff from the beginning of the project to establish the pre-application process

for permit applications. Based on the initial review comments from the agencies involved, the project team begins preparation of any permits, documentation, and agreements necessary to advance the project.

SmithGroupJJR has completed numerous permitting projects for waterfront and riverine projects throughout the Great Lakes region, and across the country.

These include:

- U.S. Army Corp of Engineers Permits (Section 404 of the Clean Water Act, and Section 9 and 10 of the Rivers and Harbors Act)
- EPA Water Quality Certificates (Section 401 of the Clean Water Act)
- National Historic Preservation Permits (Section 106 of the National Preservation Act)
- Federal Threatened and Endangered Species Review
- FEMA Permitting
- DNR Shore Protection Permits
- DNR Submerged Lands Lease Agreements
- DNR Coastal Management Consistency Statements
- Stormwater Permitting per the National Pollutant Discharge Elimination System (NPDES)

Environmental Analysis and NEPA

SmithGroupJJR specializes in assisting clients in complying with the National Environmental Policy Act (NEPA) and its requirements for Environmental Assessments and Environmental Impact Statements. Our approach is comprehensive in integrating the NEPA process with other state and federal laws, regulations and permit requirements. Our wide range of professional experience provides for effective assessment of issues including wetlands, natural resources, air and noise quality, environmental justice, community impact analyses, socioeconomics, land use planning, and secondary and cumulative impacts.

Wetland Investigations and Permitting

SmithGroupJJR's wetlands expertise has established the firm as a leader in all aspects of wetland delineation, permitting, mitigation banking, design, construction administration, and monitoring. We have successfully and cost effectively completed over 1,900 acres of wetland design projects throughout the Midwest and offer a unique combination of

field experience and regulatory knowledge. SmithGroupJJR's experience and ability to closely coordinate with regulatory agencies can streamline the process of obtaining permits.

Our environmental and permitting expertise was key to the successful recent implementation of the Upper St. Clair River Habitat Restoration project, which blending the need to remove marine debris with improvements that included new fisheries and riparian habitat, a public multi use trail, future fishing piers, and Great Lakes access points.



St. Clair River Habitat Restoration, Port Huron, Michigan

Completing of coastal studies and assessments that will or may be required by this project, including those required by all permits and other approvals that the respondent believes will be required for this project.

Shorelines often include unique characteristics, restrictive wave conditions and varying water depths that make planning, design and safe access considerations a challenge. SmithGroupJJR addresses each project with site specific solutions that control the coastal experience, wave patterns, littoral current, and sedimentation that affect safe navigation. Our engineers perform the appropriate studies and offer innovative coastal design solutions for wave attenuator systems, sedimentation control, beaches, harbor basin and channel engineering, and dredging. Our team has extensive background in coordinating with the appropriate entities to successfully require permits and approvals to support coastal community project success.

Structural design of the new public pier will require knowledge of hydrodynamic loading from local wind-generated waves, lake swells, and flood river flows. SmithGroupJJR will perform advanced hydrodynamic and hydrologic numerical models of the

project site to determine these environmental loads. Various design criteria will be simulated to ensure continued performance of the pier for years to come.

FLEXIBLE DESIGN ALTERNATIVES

Consideration of options in all elements of projects such as that to be addressed through this RFQ.

Fulfilling our clients' needs with superior design services that lead to memorable design products is of central importance in SmithGroupJJR's design philosophy. For us, every project is distinct and its solutions lie in the needs and mission of the client, underlying community and facility economics, the character of the site, and the nature of the surrounding community. Exploring options, considering all the opportunities, and addressing site constraints are critical to the successes of a project similar to this. We bring an approach that collectively and collaboratively brings to the table various options for client review and allows the process of design to mature.

One consistent characteristic of our work is our interest in understanding the broader context of a given project, thoroughly understanding complex program elements, and finding solutions that successfully integrate a broad range of ideas in a unified design that fits the character of the community.

Considering and designing various forms of passive and active recreation to ensure that the design reflects a menu of realistic, safe and appropriate uses and appeals to a broad range of users.

Fulfilling the needs of the project, client, and community requires astute flexibility and consideration of the end user. Not one use can address all individuals. Through a creative design process, SmithGroupJJR has the experience and ability to create a safe and comfortable space that encourages diverse interaction points, activity nodes, and reflection space. Designing for the various needs of different users is critical and a key point to the success of an active and inspiring space along the waterfront.

In our work at Milliken State Park and Harbor we were asked to integrate a complex and lengthy list of programmed space and activities on the park site, including, for example, passive activities such as bird watching, active groups of children learning through

experiential activities, and formal events such as weddings. Given a site that is limited in size, we successfully planned for a series of multi-use spaces that allow for the layering of program elements in an organized coherent way.



Milliken State Park + Harbor, Detroit, Michigan

UNIVERSAL DESIGN + SAFETY

Applying principles of universal design to ensure universal access so that people of all ages, needs and abilities can access, use and enjoy to their fullest the end product of similar design projects.

SmithGroupJJR is dedicated to universal accessibility in public spaces and strives to incorporate seamless integrated solutions in providing access for all. We focus on client-driven, user-inspired designs that blend social spaces, functionality, and usability into a space making it a comfortable and enjoyable place to be. Designing successful and accessible environments involves a high level of specialized expertise. Creating great outdoor places for those with physical or cognitive impairments requires an important understanding of the relationship between the individual and the physical environment. SmithGroupJJR has taken various approaches to creating effective solutions and we are passionate about design that provides a sense of confidence, seamless integration, and encourages all users to feel comfortable and included.

Assessing and ensuring that user safety is considered and addressed to reduce or eliminate risks and that facilities and facility users are readily accessible to first responders or other emergency personnel.

SmithGroupJJR puts safety as a number one priority on every project we are involved in. Safety in regards

to shorelines, piers, and Great Lakes facilities is paramount, and dictates a judicious and conservative approach. Our experience with projects similar to this allows us the understanding and knowledge of how to appropriately address safety needs, eliminate unnecessary risks, and ensure that all user types are considered in the case of emergencies.

INTERPRETIVE + EDUCATIONAL SIGNAGE

Using various forms of media and other resources to educate and inform users of similar facilities about: proper use and care of the facility; safety precautions and procedures; how to use the facility to maximize enjoyment; such as instructions/directions about how and when to fish for various species; how users can become good stewards of the facility, Grand Traverse Bay and Lake Michigan; natural resource information about the Bay, Great Lakes, etc. so that the facility is a hub of useful and interesting information.

Our team understands the important aspects of interpretive and educational signage. Flexibility, knowledge of technology, and expressing a message or brand through interpretive signage creates a sense of place and discovery. The development of your interpretive message begins at the first project meeting and flows through the entire process. Successful implementation involves seamless integration of the interpretive and educational message into the project as the program develops. A critical understanding and ability to incorporate these elements is what creates a cohesive, engaging, and thoughtful message for users to partake in.



Interpretive Design at Milliken State Park wetland overlook

CONSTRUCTION DOCUMENTS + DESIGN

Developing and assessing design and construction options for facilities such as those included in this RFQ.

Developing design specifications and construction drawings and details in a manner that ensures the success of this and similar projects.

The SmithGroupJJR team is always committed to a high level of quality and client satisfaction. Our clients are entitled to consistent, high-quality design documents that are developed with ingenuity, creativity, and timeliness, yet grounded in their program, budget and schedule. On every project, SmithGroupJJR uses a number of project controls to ensure that both design and document excellence is achieved while ensuring that budgets and schedule are met. Great design and great projects are the result of an interactive, interdisciplinary process that blends science with art and balances beauty with function. SmithGroupJJR embraces this philosophy through a collaborative work environment, encouraging employee creativity while assuring the integrity of the product.

An approach to successfully ensure the development of quality design documents requires open communication and a strong sense of shared purpose, including regular communication, flexibility of team members from the consultant and client side to review draft design documents and specifications, and organization of the design and construction documents to work within the constraints of the waterfront community and expertise of local contractors.

Quality Control/Quality Assurance

SmithGroupJJR has a formalized Quality Control program in place to provide employees a procedural framework to follow for each project so that every product is reviewed by senior technical staff prior to it being issued to the client. As part of this process, SmithGroupJJR employs a Quality Assurance Review. Projects are evaluated at, and professional expectations are set for, each step in the design process.

Specification Writing Credentials and Experience

SmithGroupJJR's project specifications combine our own firm master specifications and AIA/Arcom MasterSpec. SmithGroupJJR's masters, shaped over generations of practice, proactively address construction-phase issues in a way that only extensive field experience can, including contractual as well as quality concerns. In addition, SmithGroupJJR has subscribed to MasterSpec, the most widely-used construction specifications service in the world. SmithGroupJJR uses MasterSpec to ensure that our firm masters are up-to-date with industry standards and practices.

DEADLINES + PHASING

Meeting project deadlines for projects that have multiple phases and include significant opportunities for public participation.

SmithGroupJJR has built its reputation and practice on the proven ability to deliver quality planning both on time and within budget. At the beginning of every project, a schedule is established detailing interim and formal milestones, team meetings and points of coordination and review. The schedule for individual tasks is integrated with those of larger tasks so that all efforts can be monitored. The schedule is continuously updated so that all team members are informed of and comply with coordination and quality review milestones. This proactive management approach allows the team to respond quickly to any changes that impact the schedule over the course of the project.

Our ability to guide a complex planning process is one of our greatest strengths. The key to successful facilitation is developing a framework for decision-making that allows stakeholders to meaningfully assist in developing and implementing goals. These opportunities occur throughout the process and our team incorporates the varying levels of design and phasing into this accordingly. Stakeholders and the community are encouraged to provide input at all stages of the project.

SmithGroupJJR takes a lead role in defining, clarifying, and communicating this decision-making framework to all project partners and stakeholders. The decision-making and input process during

the early phases of the planning process should be carefully choreographed and developed to maximize input and gain community support.

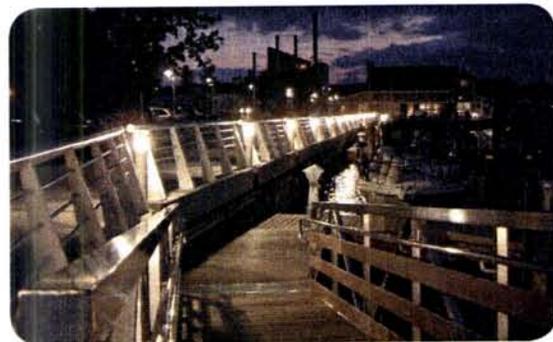
MANAGEMENT COSTS

Identifying all future asset management costs including anticipated operation and maintenance the City to incur should the pier be constructed.

Creating new public facilities does impact municipal budgets for maintenance and upkeep of facilities. The design team needs to recognize the long term impacts of the work being designed, and consideration must be given at each stage of the design process.

During the initial phases of design the SmithGroupJJR team will work with City staff to assess maintenance practices such as snow removal, as well as understand your preferred materials that help to standardize long term requirements. For instance, as lighting design has evolved to incorporate energy saving technologies, which products or approaches has the City used successfully? How many different lighting sources and lamp types are acceptable? As the design process continues, the detailing and material selection need to be carefully considered to insure that initial cost, function and appearance are coupled with long term maintenance as design criteria.

Long term management related to equipment replacement and repair, security, and events are also important considerations. SmithGroupJJR has assisted clients in understanding the long term implications of these issues and developed specific management and maintenance guidelines for projects such as the Detroit Riverwalk, helping to insure the success of the facility, long after construction concludes.



SCOPE + APPROACH /

For discussion purposes we have prepared a draft scope of work and project approach, which can be modified as needed to meet your expectations.

PHASE ONE ///////////////

SITE INVESTIGATION + SCHEMATIC DESIGN

Task 1.1: Environmental + Site Investigation

The SmithGroupJJR Team will conduct a preliminary review of the available environmental and planning data and determine the implications on the design efforts. This review will include:

Data Collection and Review: Review in detail the environmental investigations and past planning and design efforts completed to date.

Site Visit: Review and document existing site conditions, including plant materials, habitat values, slopes, existing built structures, walls and bridges, paths, and site amenities in the context of evaluating the potential impacts, and feasibility, of the proposed improvements.

Task 1.2: Site Survey

The SmithGroupJJR Team, led by Gordie-Fraser, will complete topographic surveying services as follows:

Topographic Survey: A topographic survey of the project area will be completed in order to document the current existing conditions.

Bathymetric Survey: A bathymetric survey will be completed to provide the current underwater topographic contours within the near shore environment, as needed for hydrographic modeling and preparation of construction documents.

Task 1.3: Establish a Public Outreach Campaign

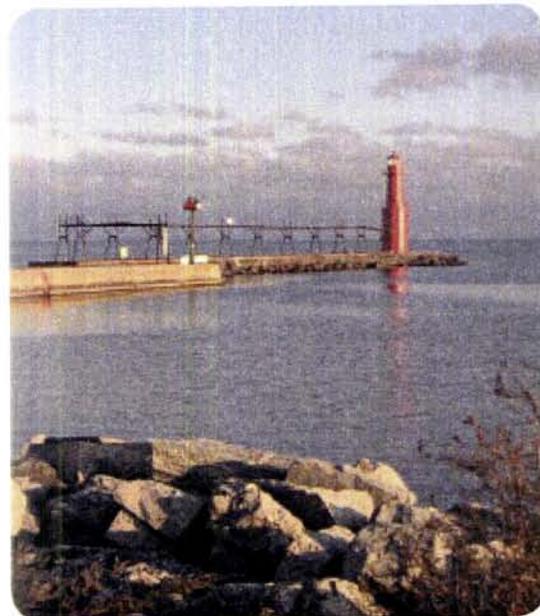
The SmithGroupJJR Team will conduct a public outreach campaign that includes public meetings, tours of the site led by our team, coordination with local news outlets, and a social media outreach plan.

To help facilitate public involvement, we suggest that you form a Steering Committee made up of commu-

nity citizens, public officials, agencies, organizations, and religious/educational institutions, and other stakeholders to represent the community's interests and provide critical input and guidance. We are also proposing to involve individual property owners, stakeholders, and the general public, in a meaningful way through active listening in personal interviews, presentations and design workshops. We use a variety of potential engagement techniques to allow for all participants and groups to be comfortable in providing input.

Our team is experienced in creating a social media presence through a comprehensive Facebook page, as well as Twitter and other social media outlets. We will post, and link to, project updates, plans, and exciting information to keep the community involved in the process. We have used this approach as a low cost method of tapping into social media, communicating important project information, building community enthusiasm, and soliciting public input.

Our team also collaborates with MindMixer, a social media platform built to streamline the dispersal of project information, facilitate community involvement, and sustain engagement and momentum in a project.



Task 1.4: Kick-off Workshops + Interviews

The SmithGroupJJR Team will travel to Traverse City for a site visit and set up a project workstation within a local office or retail space, preferably within the downtown area. During this initial site visit, we will accomplish a series of activities, as follows:

1. Conduct a Kick-off Meeting with the City staff and the Steering Committee to review project requirements, schedule, and scope of work, and develop a set of project goals and objectives.
2. The consultant team will lead a tour of the project area with City staff and the Steering Committee members. We have found that looking at a familiar place with a larger group can spark productive discussions and fresh observations of the physical environment that often surprise participants.
3. The first public workshop will kick-off broader public engagement for the project. This first workshop will (1) provide an overview of the planning process and schedule; (2) incorporate a visioning process to let the community share their long-term goals and objectives; (3) review draft existing conditions analyses, and (4) use a feedback activity to broadly discuss issues and opportunities from the public's perspectives.



Task 1.5: Coordination with Regulators + Funders

The SmithGroupJJR Team will coordinate a meeting with key regulatory and funding agencies to discuss the potential pier and waterfront improvements. The meeting will cover the planning leading up to the current proposal, the current opportunity to improve habitat and public access to the water, and input from the perspective of the regulatory and funding agencies and organizations.

Task 1.6: Schematic Design Plan

To confirm the scope and design direction of the project, the SmithGroupJJR Team will prepare a Schematic Design Plan. Our efforts will include the following:

Design Workshop: A design workshop to be held in the Traverse City area to collaboratively develop at least three waterfront improvement alternatives for the project area.

These alternatives will consider:

- Pier recreational uses and amenities,
- Paths connecting to downtown, Clinch Marina and Park, and the larger TART system,
- Fishing and water access
- Habitat creation opportunities

The workshop will be structured as an interactive meeting that allows participants to openly comment on the ideas presented, help in the development of new ideas for the plan, and offer input as to preferences. We anticipate that the result of the workshop will be a series of design sketches illustrating the design ideas, a written summary of the conclusions of the group, and a listing of outstanding design issues that require additional study.

Weather permitting, we often like to tour the site with the public as a precursor to the workshop, or conduct the workshop on site.

Alternatives Evaluation: The alternative waterfront improvement plans which result from the workshop will be evaluated for positive and negative attributes, including:

- Anticipated project costs
- Potential environmental and related impacts and challenges
- Benefits to community relative to recreational use of habitat and open water use
- Habitat creation value
- Permitting challenges
- Long range challenges for sediment management

A meeting will be held City staff to review the evaluation, determine the best plan for moving forward, and develop a preliminary list of project budget priorities.

Schematic Design Plan and Workshop: Based on the results of the alternatives evaluation a Schematic Design Plan will be prepared that illustrates the desired harbor improvements, landforms, habitat areas and features, and recreational amenities. The graphic plan illustration will be supplemented with example photographs, cross sections, and sketches to communicate the design and engineering intent. A draft Statement of Probable Construction Costs will also be developed to understand the budget implications of the proposed plan.

The Schematic Design Plan will be accompanied by a memorandum style report which summarizes the proposed actions, anticipated environmental impacts, and potential mitigation measures.

The Schematic Design Plan will be presented to the City and public at a workshop style meeting in Traverse City to review and get input on the plan and anticipated costs.



PHASE TWO

PRELIMINARY DESIGN

Task 2.1: Hydrological Modeling

Each coastal zone is subject to a different combination of dynamically unique characteristics which can make planning, design, and safe access considerations a challenge. SmithGroupJJR approaches each project with site specific solutions that attempt to compliment the challenges presented by the wind, wave, longshore current, and sedimentation issues which affect each project site. Our engineers have extensive knowledge and experience in working with nature to develop innovative design solutions for wave attenuation, sediment control, beach and shoreline stabilization, coastal structure design, harbor agitation improvement, dredging mitigation, as well as a number of other common coastal zone issues. Our waterfront team has extensive background in coordinating with the appropriate entities to successfully acquire the required permits and approvals to support coastal community project success.

For the development of a new public pier, a full understanding of coastal loading will be required. This metocean study will include an assessment of site specific environmental conditions and an extremal analysis to forecast future extreme events which will form the basis of the structural design. It is likely a review of littoral transport will be required by permitting agencies to mitigate morphological changes directly related to the construction of a new shoreline pier. SmithGroupJJR's engineers have performed numerous studies of this nature throughout the Great Lakes region and are well qualified to efficiently and expeditiously provide design criteria and solutions in line with regulatory practices.

Task 2.2: Preliminary Design

Based on the approved Schematic Plan, input from the regulatory and funding agencies, and the results of the modeling, the SmithGroupJJR Team will prepare a set of Preliminary Design Plans that illustrate the waterfront improvements in sufficient detail to communicate the design intent, measure quantities for cost estimation, and support permit applications and the pursuit of grants.

We anticipate the following drawings will be required as part of this set of preliminary design documents:

- Cover Sheet
- Existing Conditions
- Site Preparations, including typical Dredge and Deposition Cross Sections (if required)
- Grading Plans
- Typical Cross Sections of the pier and site improvements
- Landscape Plans illustrating typical landscape treatments and the location of habitat features

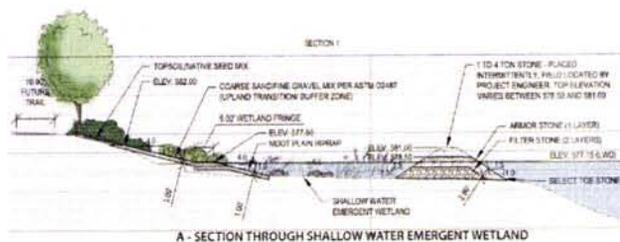
The plans will be supplemented with a Statement of Probable Construction Costs as well as an Implementation Phasing Plan to assist the City in understanding the potential costs of the construction project and the opportunities for phasing implementation over time if necessary.

Task 2.3: Grant and Funding Opportunities

The SmithGroupJJR Team has extensive experience securing grant funding for waterfront projects and we can assist the City with assessing grant and funding opportunities, communication with potential funding sources about the project, and preparing grant and funding applications.

Task 2.4: Regulatory Permitting

Assuming that the project is funded and the pursuit of permits deemed appropriate, a joint permit application will be prepared for and submitted to the USACE and the MDEQ. The permit application will cover all of the anticipated improvements to the waterfront, habitat area, and park amenities based on the completed Preliminary Design Plan. Other regulatory permits will be prepared and submitted as required.



PHASE THREE

FINAL DESIGN

Task 3.1: Construction Documents

Final Design: Perform civil engineering and landscape architectural services to finalize pier plans, dredging plans, grading and earthmoving, cross sections and details for the pier, shoreline/slope stabilization and habitat creation, landscape plantings, paths and boardwalks, interpretive signage, and displays.

We anticipate the following will be required as part of this set of construction documents:

- Cover Sheet
- Existing Conditions
- Site Removals and SESC
- Layout Plans
- Grading Plans
- Construction Details, including pier, habitat features and shoreline stabilization
- Detailed Habitat Cross Sections
- Landscape Plans
- Landscape and Habitat Details
- Bidding and Contract Documents
- Opinion of Probable Construction Costs

Task 3.2: Bidding Phase

During the bidding phase, the SmithGroupJJR Team can:

- Assist the City with advertisement for bids.
- Provide the City with a list of potential bidders experienced in similar construction to be added to their list of local bidders.
- Attend the Pre-Bid Meeting and Bid Opening.
- Issue addenda to the bid documents as necessary to clarify document intent.
- Review bids and offer recommendation of action.

PHASE FOUR

PROJECT CONSTRUCTION

During the construction phase of work, the City would typically be responsible for the day-to-day administration of the contract. The SmithGroupJJR Team is available to provide the following construction administration tasks:

Submittals: Review and approve shop drawings, samples and other data which the Contractor is required to submit.

Pre-Construction Meeting: Participate in the pre-construction meeting.

Change Documentation: Prepare and distribute bulletins, change orders, and supplemental instructions (e.g., responses to RFI's) including clarifications and progress reports as required to address drawing clarification, field conditions and minor modifications to the work.

Site Reviews: Make visits to the site at intervals appropriate to the various stages of construction in order to observe the progress and quality of the various aspects of the Contractor's work.

Payment Applications: Review payment application requests submitted by the Contractor to determine if the work completed and the worked invoiced are reasonably matching.

Close-out Inspections: Conduct a site review at substantial completion to determine if the work is substantially complete and prepare a punch list of uncompleted or unacceptable work items. Upon notice that all punch list items are complete, a final site review will be held to determine if the completed work is acceptable.

Record Drawings: Receive and assemble record drawings (prepared by the Contractor(s)) and deliver them to the Client after completion of construction.

Other Construction Services: Given the strong local members of our team, we are prepared to provide a full range of construction administration services, including geotechnical engineering, daily site inspection, construction survey, and materials testing, and we can tailor our scope of work of meet the needs of the City.



PROJECT EXPERIENCE /

The SmithGroupJJR design team recognizes that this project is a unique opportunity to create a **meaningful and exciting waterfront PLACE** for Traverse City and its visitors. The new pier will become a safe, enjoyable and captivating amenity, create unique fishing opportunities, improve existing habitat, educate the community, and provide universal accessibility to the waterfront. Further, the pier will provide a place for a range of community activities, create a “book end” that balances Clinch Park and Marina, highlight the Boardman River, and connect to downtown.

Our team has extensive background with similar pier design and improvement projects. Our multi-disciplinary team, including environmental specialists, civil engineers, and landscape architects, will approach the pier project with **great enthusiasm, thoughtful design, and relevant experience**. We are confident our team will offer a unique perspective to help you create an exciting, compelling, and engaging destination for the City of Traverse City.



Sim's Park Fishing Pier | Euclid, Ohio



Before



After



The Sims Park Fishing Pier is one a key milestone for the Euclid community, and illustrates the City's commitment to increasing public access to Lake Erie. Completion of the pier has helped advance a broader vision for their waterfront. SmithGroupJJR, who also prepared the community's Waterfront Improvement Plan, worked with area residents to re-purpose the substructure of the existing 150 foot storm sewer outfall structure and replace it with the new fishing pier. SmithGroupJJR provided the design, engineering and permitting for the new pier and worked with the City to identify and secure grants to assist with project development.

The new pier provides universally accessible fishing access to deeper water. The arching shade structure

maximizes the shaded areas, and serves as the gateway to the pier's lower platform near the terminus. Stone surrounding the lower platform is designed to break waves and serves as an ice-breaker. Bollard lights and fixtures are integrated into the railing for low-level lighting that respects night-skies and will be powered by a vertical-axis helical wind turbine at the terminus.

Hardy plants installed by area community groups will serve as a backdrop for benches that sit atop the raised planters walls and local artisans were engaged to design and forge the bronze medallion commemorating the pier's dedication. Trail restoration includes over 150 linear feet of a concrete and steel bridge spanning a new inlet, which is designed to support emergency and maintenance vehicles.

SMITHGROUP JJR

Navy Pier Revitalization | Chicago, Illinois



A favorite destination for residents and tourists alike, Navy Pier is considered one of Chicago lakefront's crown jewels. The Metropolitan Pier and Exposition Authority contacted SmithGroupJJR to make the most of what the city knew was an underutilized asset: Navy Pier, a historic structure that stretches from the downtown lakefront 3,300 feet out into Lake Michigan.



SmithGroupJJR prepared engineering studies and design criteria to preserve existing piles, stabilize the perimeter dock wall, and add a new north dock for mooring. The 1916 pier was in need of repair and restoration, paving the way for its rebirth as a major tourism venue and favorite destination along the Chicago lakefront. Project restoration efforts included a new north and south overlook platforms; stone revetment stabilization in depths up to 30 feet; foundation underpinning for the historic Head House; pump station; paving and repair renovation work.

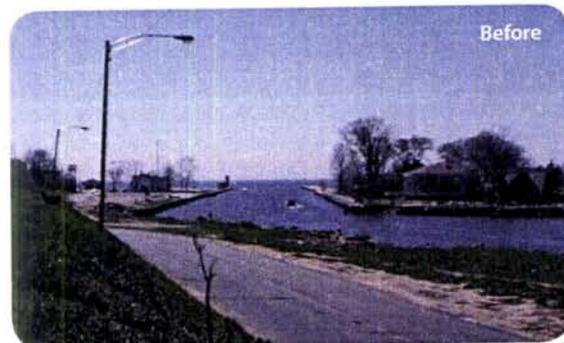
SMITHGROUP JJR

South Haven Marina Park + Riverfront Improvements | South Haven, Michigan

The goal of the project was to establish an implementation-oriented waterfront plan for the city. The planning and design emphasized utilizing the riverfront of the Black River as a linear park linking the beaches of Lake Michigan to the increasingly vital downtown center. The Marina Park included a 40-slip marina for visiting or transient boaters and a large public pier for gathering, fishing, and boat watching.

Working with City officials and the community, SmithGroupJJR used public input, coastal analysis, and creative planning to create a final design, which addresses elements such as park and recreational uses, coastal engineering, economic development, and environmental issues.

The centerpiece of the linear park is the small boat basin located near the harbor entrance. A unique shore protection solution incorporating a wave attenuating quarry stone revetment was designed to protect the park and marina from wave surge from Lake Michigan. The former lumber mill site was transformed into a popular community gathering spot for art festivals, concerts, and a farmer's market, attracting residents and tourists to South Haven's downtown riverfront.



SMITHGROUP JJR

Milliken State Park + Harbor | Detroit, Michigan

In 2002, the brownfield lands along the shores of Detroit's east riverfront, home to parking lots, cement silos, and maintenance yards, were designated as Tri-Centennial State Park and Harbor—the first urban state park in Michigan and the 97th park in the system. Since renamed the William G. Milliken State Park and Harbor, the park and its 52-slip transient marina provides Michigan's 5 million residents access to the state's many natural resource-based recreational opportunities and serves as a gateway experience to the rest of Michigan's famous state parks.



SmithGroupJJR worked with the Michigan Department of Natural Resources to prepare a Master Plan for the park, and we have assisted in the design and implementation of a number of key projects, including design marina improvements, waterfront promenades, fishing access, non-motorized and multi-use paths, and a 5 acre demonstration wetland. The Master Plan integrates a comprehensive program for park improvements and activities within a limited size site by planning the facilities and spaces to be flexible and multi-functional. Integrating learning opportunities and environmental awareness information for visitors was a primary guideline for each phase of the project.



SMITHGROUP JJR

Kids Creek Tributary: A Relocation | Traverse City, Michigan

As with the previous successful Kids Creek restoration projects, this one provided a win-win situation: the tributary returned to a more natural, above-ground course while the campus benefited from additional land for expansion.

This project restored approximately 1,275 lineal feet of the tributary, improving fish passage and habitat while also improving overflow conditions and creating a green park-like buffer between the hospital campus and the residential neighborhood.



With Dan Wagner as the project manager, GFA worked closely with Munson, the Watershed Center, and the City of Traverse City. GFA provided civil design services for hydraulics, stormwater, two street bridges, utility relocation and site work support as well as construction phase services. Plans required demolition of parking lots and four buildings.

This project was part of Munson Medical Center's master plan. It is part of a larger effort being coordinated by The Watershed Center and Grand Traverse Conservation District to remove Kid's Creek from the Michigan DEQ's Impaired Waters list.



REFERENCES/

Mr. Jack Kelly, Township Supervisor

Elmwood Township
Elmwood Marina District Plan + Waterfront
Corridor Subarea Plan
Elmwood Township, Michigan
231.946.0921
supervisor@elmwoodtownship.net

Mr. Randy Maiers, President and CEO

Community Foundation of St. Clair County
Port Huron South Waterfront Master Plan
Port Huron, Michigan
810.984.4761
randy@stclairfoundation.org

Mr. William Smith, Chief Financial Officer

Detroit RiverFront Conservancy
Detroit East RiverWalk
Detroit, Michigan
313.566.8225
william.smith@detroitriverfront.org

Tom Cannon, Parks Director + Asst. City Manager

City of East Jordan
Marina + Waterfront Improvements
East Jordan, Michigan
231-536-2561
tcannon@eastjordancity.org

"The proof is in the pudding. SmithGroupJJR did an outstanding job recognizing our needs, challenges, and opportunities. They literally demonstrated this during the interview, and in the final output of the waterfront district plan they confirmed that they were the team we needed to take things to a new level."

**-Jack Kelly
Elmwood Township Supervisor**

"Building these things is not easy. There is a regular framework, a political framework, and a fiscal framework. SmithGroupJJR understood that and worked very well within the frameworks to maximize our opportunities."

**-Josh Van Lieshout
Village of Egg Harbor Administrator**

"SmithGroupJJR has always been a Johnny-on-the-Spot. I find them to be extremely personable and forthcoming, they assist in whatever way they can, and they are very prompt at returning phone calls and answering questions."

**-Scott Kluver
City of Washburn Administrator**

ANN ARBOR

CHICAGO

DALLAS

DETROIT

LOS ANGELES

MADISON

PHOENIX

SAN FRANCISCO

SHANGHAI

WASHINGTON, DC

SMITHGROUP JJR

9. Signature Page

TITLE: Traverse City Public Pier RFQ

DUE DATE: June 12, 2014 at 4:00 p.m.

Having carefully examined the attached RFQ and any other applicable information, the undersigned proposes to furnish all items necessary for and reasonably incidental to the proper completion of this RFQ.

The undersigned understands and agrees that they must be licensed to do business as Professionals in the State of Michigan.

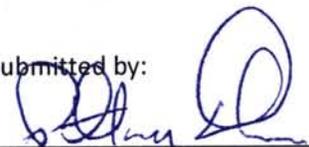
The undersigned submits this proposal and agrees to meet or exceed all requirements and specifications listed on the RFQ, unless otherwise indicated in writing and attached hereto, and acknowledges a thorough understanding of the City's Great Lakes Fisheries Trust grant agreement.

The undersigned certifies, as of the date of this RFQ, not to be in arrears to the City of Traverse City for debt or contract or is in any way a defaulter as provided for in Section 152, Chapter XVI of the Charter of the City of Traverse City.

The undersigned understands and agrees, if selected to be awarded this work, to enter into an agreement with the City to supply this work.

The undersigned understands that the City reserves the right to accept any or all proposals in whole or in part and to waive irregularities in any proposal in the interest of the City. The RFQ will be evaluated and awarded on the basis of qualifications and best value to the City. The decision criteria to be used, but will not be limited to, is qualifications, technical expertise and experience, key staff, past similar work, firm's understanding of the project scope, quality of the firm's project approach and overall capability to meet the needs of the City.

The undersigned agrees that the RFQ may not be withdrawn for a period of 60 days from the actual date of the opening of proposals.

Submitted by: 

(Signature)

Patrick M. Doherty, PE, LEED AP
Senior Vice President

734.669.2766

(Name & Title - print)

(Telephone Number)

SmithGroupJJR

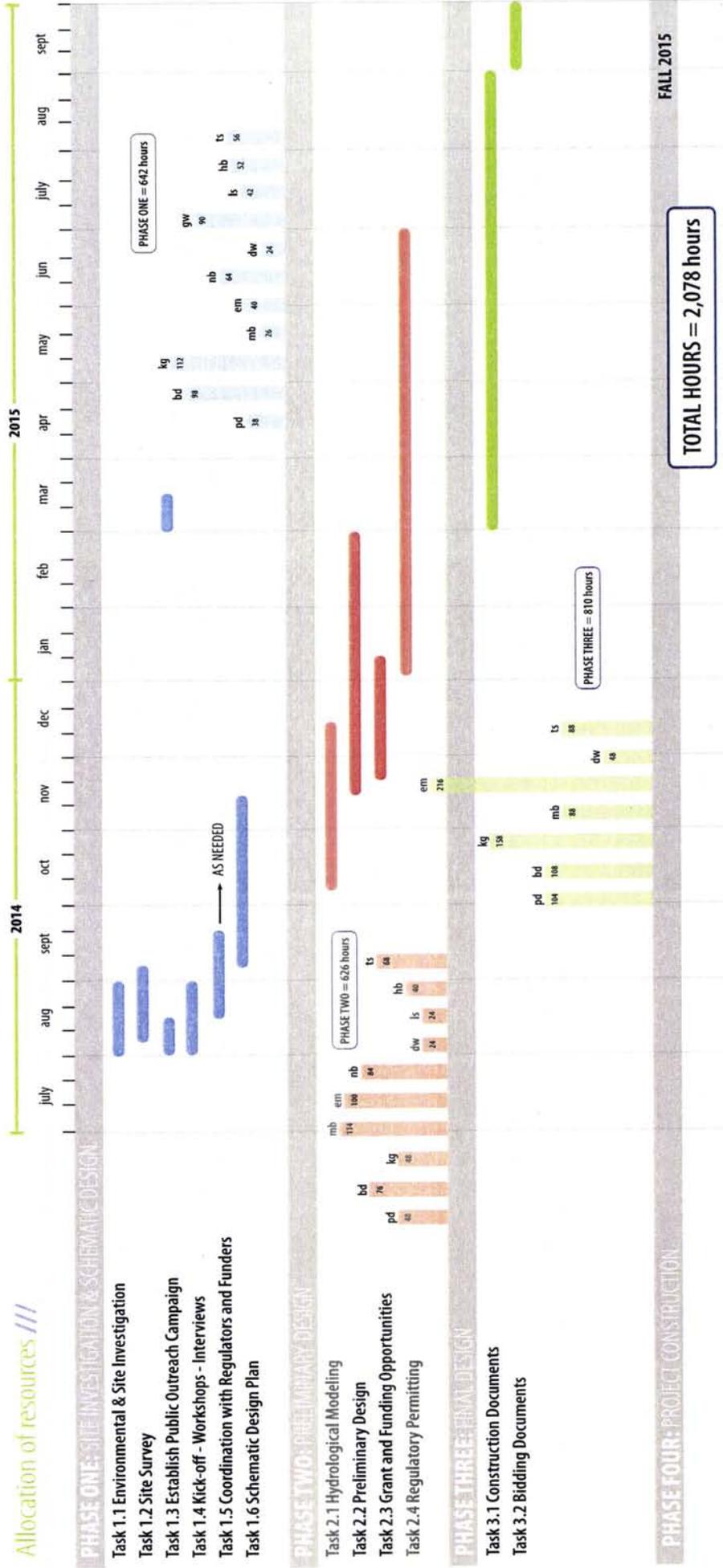
(Company Name)

201 Depot Street, Second Floor, Ann Arbor, MI 48104

(Company Address, City, State, Zip Code)

tasks + hours

Allocation of resources ///



SMITHGROUP JJR

Coastal. Community. Process.

B A Y

- salmon
- laker
- steelhead
- walleye
- smallmouth

Response to Additional Information Request
Traverse City Public Pier

JULY 16, 2014



SMITHGROUP JJR

D M A N R I V E R

imagine access

imagine a pier...

SMITHGROUP JJR

July 16, 2014

Ms. Missy Luick
Planning and Engineering Assistant
City of Traverse City
2nd Floor, Governmental Center
400 Boardman Avenue
Traverse City, Michigan 49684

Re: Traverse City Public Pier
Response to Additional Information Request

Dear Ms. Luick:

The SmithGroupJJR Team is very excited to provide you and your Selection Committee additional information for consideration as you continue the process of selecting a partner firm for the design and construction of the Public Pier at the mouth of the Boardman River.

We have organized our response in accordance with your email request dated July 11, 2014.

Based on our interview discussion with the Selection Committee, and the subsequent information request, we are convinced our Team is the right partner for the City of Traverse City. Our compelling reasoning for this is as follows:

Coastal Analysis, Design, Engineering + Facilitation: Holistic, Comprehensive + Integrated.

Our Team is organized with leadership in each of the professional disciplines required for this unique project; it is also fortified with a combined firm organization of over 800 professionals with considerable depth in each discipline.

Coastal Engineering + Hydraulic Analysis: International, National, Regional + Local Experience.

Margaret Boshek has over nine years' international experience in specialized coastal engineering and analysis as well as experience in the Great Lakes, oceans and river environments. She will be supported by Smith-GroupJJR Team members Bill Brose and Jack Cox who are internationally and nationally recognized as leading coastal engineers in the field of waterfront design. Together, Bill and Jack have over 70 plus years of combined experience across the globe and they will be directly involved in evaluating and guiding our technical design efforts. This team of coastal engineers has been responsible for tens of millions of dollars in waterfront structures and facilities construction with a proven track record of technical excellence.



Omni Talon at Detroit RiverWalk



Public Engagement + Facilitation: Creative, Engaging, Inclusive + Transparent.

Our facilitation and public engagement expertise and leadership is unmatched. Bob Doyle, who will be leading this aspect of the project, has successfully navigated dozens of communities through challenging urban and waterfront projects by being compassionate, passionate, humorous, genuine and fair. Bob believes a successful facilitator must be a part of the design process and needs the requisite keen process understanding and professional savvy to lead a community towards a successful solution, approach and consensus.

Fisheries Habitat: Great Lakes Expertise.

Although we did not stress this in our submittal or interview, SmithGroupJJR has a recognized expert in Great Lakes-specific fisheries and analysis of coastal environments to ensure beneficial habitat creation and preservation. Doug Denison has served as an expert resource to the Fisheries Trust Fund Board, is an engaged leader in the fisheries biology community and is recognized by NOAA, MDEQ and the EPA as a scientific leader in environmental planning and analysis for fisheries habitat. Doug's expertise will bring great value to our Team and will help guide our decision-making process, which will limit negative impacts to the coastal environment and allow for the integration of habitat creation into the Pier's final design.

Leadership. Commitment.

The SmithGroupJJR Team's success will be measured by the sum of its parts—each team member contributing thoughtfully to our collaborative approach for the Pier's design and technical solutions. Team synergy plus leadership that is well defined, committed and able to successfully manage the process is mandatory for a team to succeed—regardless of the level of specialized expertise. As the leader of the SmithGroupJJR Team, I have the proven expertise and leadership qualifications to deliver the City of Traverse City across the goal line by meeting and exceeding all project expectations. Also, as an engineer with decades of waterfront experience, I understand the pitfalls that may arise, how to overcome them, and how to deliver a technically-successful project.

This project is going to need a leader with these qualities and experiences and the City deserves a qualified team with strong leadership and a commitment to work with the City beyond the completion of the Public Pier. This cannot happen without a personal commitment to partner with the City from the beginning of the project through to the punch list and beyond, including a review of first-year operations and maintenance. I make this commitment to the City on behalf of myself, SmithGroupJJR and the Team we have assembled to work with you on this very important project.

I look forward to our continued discussion and stand ready to begin our partnership on the Public Pier. If you have any questions, please feel free to contact me directly at 734.669.2766 or pat.doher@smithgroupjjr.com.

Sincerely,



Patrick M. Doher, PE, LEED AP
Senior Vice President

1 | MATERIALS SELECTION PROCESS/

Evidence-Based Approach

SmithGroupJJR has decades of experience in the use and function of materials used in outdoor spaces, particularly in the difficult marine environments of the Great Lakes. Our designers and engineers use a database of built projects to research the durability and maintenance issues associated with materials used in surfaces, walls, stabilization, furnishings, planted areas and other elements in the built environment. In addition, a philosophy of sustainability is deeply rooted in our exploration and selection of materials. Life cycle cost analysis, local suppliers and natural materials are key to environmental stewardship—which has always been a cornerstone of our design approach.

Together with our built projects, we also explore and benchmark other site development within the region of a particular project to determine the impact of hot and cold temperatures, freeze cycles, humidity, wind, off-shore coastal forces and processes, lake ice and the surrounding environment's impact on the use, life and maintenance of materials.

This evidence-based approach within our design process allows us to provide owners with the information necessary to make informed decisions on suggested and recommended materials.

Coupled with our design process, SmithGroupJJR has excellent relationships with suppliers, fabricators, and contractors who deliver, create, and construct elements within the outdoor built environments. These relationships allow our designers to visit fabricators, invite suppliers and contractors to our office or client's offices to discuss, demonstrate, and inform our collective teams regarding proposed materials, fabrication techniques, and design details.

In addition, we have a core group of architects and engineers that are experts in materials technology, including experts in forensics and analysis of building systems and materials that have national and international reputations in the field of durability, function, and performance. This group serves as a resource throughout our firm and to our clients as we seek to inform our teams and owners throughout the design process.

Finally, we use our relationships with municipal facilities managers and staff to determine the aspects associated with the maintenance of built environments and public spaces. There is no better resource to inform our teams and owners regarding the maintenance of outdoor spaces than those that are charged with the maintenance and operation of these spaces.

Our company has designed and implemented hundreds of projects within the Great Lakes Region, including piers, coastal structures, sediment transport management structures, beaches, marinas, promenades, boat launches, and fisheries habitat enhancements. This experience provides us with unmatched knowledge regarding the best types of materials, material performance, and the issues associated with the operation and maintenance of built outdoor spaces in this region. Whether its stone used to stabilize marine structures; guardrails intended to provide safe access to spaces and places; surfaces of stone, pavers and concrete; or furnishings and amenities within a public space, our teams have the information and resources available to ensure our projects are designed with research, evidence, and knowledge—allowing our clients to make informed decisions with confidence.

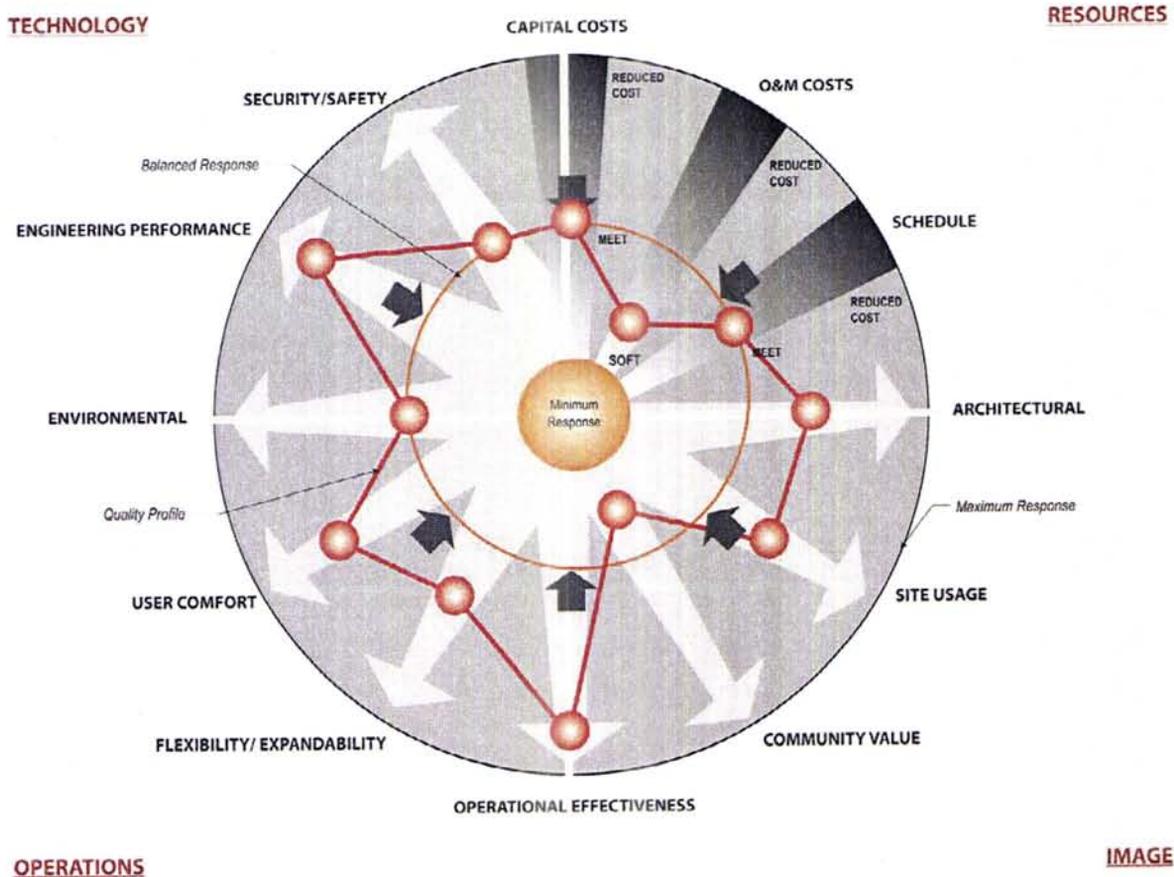
Quality Model Programming

All of this knowledge and resource data is incorporated into the early phases of our projects and is used as a benchmarking exercise to allow comparison of materials to make informed decisions. A matrix and value model is established in order to prioritize our client's desires. For example, long-term maintenance may be more important than life-safety, and long-term durability may be more important than environmental considerations.

One of the tools we use to help clients weigh options and make decisions regarding design elements, materials and project elements is a Quality Model. In an interactive workshop setting, the client's project expectations may involve design image, flexibility, functionality, technical systems performance, maintenance, budget adherence or any other issues which may shape the direction of the project.

This collaborative workshop environment between the client and SmithGroupJJR helps to build a clear, mutual understanding of design goals and objectives, how the facility should perform, what issues are the most important and, most of all, what the client ultimately wants both tangibly and intangibly.

The benefits of this comprehensive approach are a proactive understanding of facility needs in terms of function and flexibility (accommodating existing and future operational practices), and a thoroughly explored, tested program developed in a collaborated team process. The results of the Quality Model exercise is a clear path regarding project goals and performance, and allows a matrix for decision making to be used as a system of checks and balances throughout the project.



2 | WORKPLAN FOR PUBLIC ENGAGEMENT/

Based on our experience with projects of similar scale, we have estimated hours specific to managing public engagement. A detailed chart delineating the hours per team member per task is presented at the end of the section.

Each community and project is unique, and our design solutions are tailored to suit the particular needs and character of your community. Engaging the public in the design process provides us with insights into the needs and preferences of the local community and allows the public to shape the design outcomes. Our experience has taught us that the keys to a successful public engagement process include:

- Engaging the public and stakeholders early.
- Setting measurable objectives for the project against which alternatives can be evaluated.
- Using graphic illustrations to communicate ideas clearly and legibly for lay people.
- Offering genuine alternatives and leaving the “straw dogs” at home.
- Listening to and respecting all participants and discovering the kernel of sound thinking.
- Being analytical, connecting ideas together and illustrating logic.
- Showing your passion, sincerity, and humor!

For over 50 years, our site planning practice has demonstrated that the value of involving the public directly in the process of planning and design will successfully result in the following:

- Design solutions that reflect the direct input of your citizens and fit the character of the community.
- Strong community support for the plan—because they helped shape it!
- Approval and implementation of the plan that is less contentious and faster moving.
- Program elements and connections to the community that reflect the real needs and aspirations for the facility.

The following text consolidates the primary points of public interface originally provided in the “Scope and Approach” of our *Innovative Waterfront Design* qualifications dated June 12, 2014.

PLANNING PHASE / 233 hours

SITE INVESTIGATION + SCHEMATIC DESIGN

Task 1.3: Establish a Public Outreach Campaign 36 hours



The SmithGroupJJR Team will conduct a public outreach campaign that includes public meetings, tours of the site led by our Team, coordination with local news outlets, and a social media outreach plan. The format and content of the workshops that are outlined below will be used as a starting point for discussions with City staff and the Steering Committee as we refine a public outreach campaign with you.

Before the public engagement begins, we will work with City staff to prepare a detailed and specific outline of the elements of the outreach campaign, including:

- Meetings and workshop summaries noting meeting objectives, presentation content, engagement tools and exercises to be used, and logistical requirements (e.g., room size and set up).
- Social media/web-based approach, including specifics on how, when and what content will be posted; who will monitor and organize public input; and how responses may be prepared.
- A strategy for working with the local media to promote upcoming meetings/events and accurately communicate project progress.

To help facilitate public involvement, we suggest that you form a Steering Committee made up of community citizens, public officials, agencies, organizations, religious/educational institutions, and other stakeholders to represent the community's interests and provide critical input and guidance. We are also proposing to involve individual property owners and stakeholders, as well as the general public, in a meaningful way through active listening in personal interviews, presentations, and design workshops. We use a variety of potential engagement techniques to allow for all participants and groups to be comfortable providing input.

Our Team is experienced in creating a comprehensive social media approach/presence through the use of Twitter, Facebook, YouTube, LinkedIn, Instagram and Pinterest, among others. We will post, and link to, project updates, plans, and information (such as the results of a community preference survey) to keep the community involved in the process. We have used this approach as a low-cost method of engaging the community by soliciting public input, building community enthusiasm, and communicating important project information. These approaches can easily be incorporated into the City's website.

We typically suggest using social media and web-based sites to announce upcoming public meetings, present design documents at key milestones throughout the process, post preference surveys and interactive maps, provide meeting summaries to communicate the project direction, and suggest other forms of input such as contact information for key Team members (if appropriate).

Our Team also collaborates with MindMixer, a social media platform built to streamline the dispersal of project information, facilitate community involvement, and sustain engagement and momentum in a project. If requested, we could assist the City in the design, launch, and maintenance of a project website on the MindMixer platform.

TASK 1.4: Kick-off Workshops + Interviews 75 hours

The SmithGroupJJR Team will travel to Traverse City for a two-day site visit and set up a project workstation within our local team member's office or a location suggested by City staff. During this initial site visit, we will accomplish a series of activities, as follows:

1. Conduct a Kick-off Meeting with the City staff and the Steering Committee to review project requirements, schedule, and scope of work, and develop a set of project goals and objectives.
2. The Team will lead a tour of the project area with City staff and the Steering Committee members to re-familiarize everyone with existing conditions that will influence the planning and design efforts. We have found that looking at

Results for #popupworkshop

Jessie Stachowiak
2 wagons, 4 maps of #AnnArbor, 7 facilitators. ONE day full of awesome public input! #A2streetplan #popupworkshop



Ann Arbor DDA
Looking for your feedback on the #A2StreetPlan #PublicWorkshop
We'll be walking around downtown June 12th #AnnArbor #A2DDA #popupworkshop

Ann Arbor DDA
Starting to get some great feedback on the #A2Streetplan in Kerrytown!



a familiar place with a larger group can spark productive discussions and fresh observations of the physical environment that often surprise participants.

3. The first public workshop will kick-off broader public engagement for the project, including (1) providing an overview of the planning process and schedule; (2) incorporating a visioning exercise to let the community share their long-term goals and objectives; (3) reviewing draft existing conditions analyses, and (4) using a feedback activity to broadly discuss issues, opportunities and priorities from the public's perspectives.
4. Meet with the Steering Committee following these meetings to review and discuss the findings and establish a direction for next steps.
5. Perform Pop Up Workshop community outreach efforts to inform the design and programming as well as obtain other feedback to inform the project Team.

Task 1.6: Schematic Design Plan

122 hours

Design Workshop

A design workshop to be held within our local team member's office or a location suggested by City staff to collaboratively develop at least three waterfront improvement alternatives for the project area.

These alternatives will consider:

- Pier recreational uses and amenities.
- Paths connecting to downtown, Clinch Marina and Park, and the larger TART system.
- Fishing and water access.
- Habitat creation opportunities.

The workshop will be structured as an interactive meeting that allows participants to openly comment on the ideas presented, help in the development of new ideas for the plan, and offer input as to preferences. The goal of the workshop will be to gain an understanding of the range of design and planning ideas possible, their relative merits, and help reach consensus for further refinements to the Pier project.



Tools we use to solicit input from the public participating in the workshop may include:

- Small group/hands on interaction.
- Charrette style idea generation with design professional and public collaboration.
- Preference exercise to help define the design character, program elements, and/or alternative design ideas desired.
- Informal one-on-one open house style interaction with the design Team members.

We anticipate that the result of the workshop will be a series of design sketches illustrating the design ideas, a written summary of the conclusions of the group, and a listing of outstanding design issues that require additional study.

Weather permitting, we often like to tour the site with the public as a precursor to the workshop, or conduct the workshop on site. SmithGroupJJR has recently implemented a new form of community outreach called Pop Up Workshop. These are literally on-the-spot community gathering efforts in which we carry our visioning and design tools into the community to gather informal, immediate and organic feedback about the project, its use, and the desired program elements and outcomes. Typically, we perform these efforts in areas of high pedestrian activity and near the project site. These workshops are also a good opportunity to increase dialogue

about the project via social media. During our most recent Pop Up Workshop in downtown Ann Arbor, we conducted a successful Twitter campaign using pictures and #PopUpWorkshop. The tweets were “favorited” and “retweeted” by SmithGroupJJR, the client and other team members.

Schematic Design Plan and Workshop

The Schematic Design Plan will be presented to the City and public at a *workshop-style* meeting in Traverse City to review and gather input on the plan and anticipated costs. As the design work is refined at this stage in the process, there is still considerable room for meaningful public input. Typically a workshop at this stage includes a brief presentation of the design refinement and alternatives being considered, followed by an interactive set of exercises to engage the participants (as outlined above for the previous design workshop). The design Team will prepare plans, perspective views, and diagrams to help clearly communicate to the public the design intent and the alternative choices available.

The key objectives for the workshop will be to:

- Confirm that the design reflects the input provided in the previous design workshop.
- Assess priorities for implementation, i.e., what matters the most to the community?
- Help define the character and materials for the design through consideration of alternatives.
- Provide information as to the anticipated next steps for the project.



DESIGN, BID + AWARD **PHASE / 48 hours**

PHASE 2: PRELIMINARY DESIGN

PHASE 3: FINAL DESIGN

PHASE 4: PROJECT CONSTRUCTION

During these phases, our work related to public engagement will include meeting with the public and elected/appointed commissions and updating the web-based content on an as-needed basis to provide information to the community on the progress of the project.

Hours Delineated for Public Engagement

| | SmithGroupJJR | | | | | Gourdle-Fraser | | TOTAL HOURS |
|--|---------------|-----------|------------|-----------|-----------|-----------------|-----------|-------------|
| | B. Doyle | P. Doher | K. Gibbons | M. Boshek | Staff | H. Harris-Brady | D. Wagner | |
| SITE INVESTIGATION AND SCHEMATIC DESIGN | | | | | | | | |
| 1.3 Establish a Public Outreach Campaign | | | | | | | | |
| Draft Workshop Outlines/Web Strategy | 6 | 1 | 1 | | | 2 | | 10 |
| Meet with Staff and Steering Committee to Review | 3 | 2 | | | | 4 | | 9 |
| Coordinating Web-based Platform | | | 4 | | | 2 | | 6 |
| Managing Web-based Content | 1 | | 6 | | | 4 | | 11 |
| 1.4 Kick-off Workshops and Interviews | | | | | | | | |
| Kick-off Meeting | 6 | 4 | | | | 4 | | 14 |
| Project Site Tour | 4 | 2 | 6 | | | 2 | | 14 |
| Workshop #1 Organization and Prep | 4 | | 7 | | 4 | 8 | | 23 |
| Public Workshop #1 | 4 | 4 | 4 | | | 2 | 2 | 16 |
| Steering Committee Meeting to Review Results of Workshop | 4 | 4 | | | | | | 8 |
| 1.6 Schematic Design Plan | | | | | | | | |
| Workshop #2 Organization and Prep | 6 | | 6 | 4 | 4 | 16 | | 36 |
| On Site ("Pop-Up") Workshop #2A (afternoon) | 4 | 4 | 4 | 4 | | 2 | | 18 |
| Design Workshop #2B to Explore Alternatives (evening) | 4 | 4 | 4 | 4 | | 4 | 2 | 22 |
| Workshop #3 Organization and Prep | 8 | 1 | 4 | | 4 | 10 | | 27 |
| Schematic Design Plan Workshop #3 to Refine Plans | 12 | 6 | | | 1 | | | 19 |
| PRELIMINARY and FINAL DESIGN | | | | | | | | |
| Meetings and Public Updates | 12 | 8 | | | | 12 | 2 | 34 |
| Managing Web-based Content | 2 | | 4 | | | 8 | | 14 |
| SUBTOTAL OF HOURS by professional | 80 | 40 | 50 | 12 | 13 | 80 | 6 | 281 |

Please note that many of the hours used to prepare for public meetings and participation also benefit other phases of the project, such as Concept Development and Schematic Design, to ensure an efficient use of project dollars and the City's resources. In addition, our project team leader has been allocated a certain budget of hours; however, he is committed to the project, as required, and it is anticipated his level of effort will exceed the hours allocated, without additional expense to the City, to ensure the project's success.

3 | CONNECTIVITY + PIER ACCESS/

For the Traverse City Public Pier to become a successful public space, it needs to have a clear, legible, and logical connection to the community and its key assets.

Connecting the waterfront to the community in Traverse City is a challenge. The *Your Bay, Your Say* waterfront master planning process outlined several key strategies for improving connectivity, including improvements to the crossing of Grandview Parkway at Union, Cass, and Park streets and improving the non-motorized connection at the Boardman River under the Grandview Parkway Bridge.

The Public Pier is another opportunity to help activate the waterfront on the eastern end of downtown while improving the waterfront connection to downtown. The best way to reach this potential and successfully provide clear access is to consider a range of alternative ideas and vet them through an open and transparent public engagement process. Our design methodology is rooted in the exploration of comprehensive, creative, and innovative alternatives taking into account best practices from around the globe, community preferences, and basic cost/benefit analysis.

Technical Challenges:

Design Opportunities

The grade difference between Grand View Parkway and the water of the bay is a key challenge; however, the SmithGroupJJR Team believes the challenge is not as significant as it may appear, and with some creative problem solving it can be addressed seamlessly.

Based on the preliminary typographic information collected to date by our Team, we know there is about 20 foot of vertical elevation difference between the water and the bridge. Assuming that the primary elevation of the pier is about 8 feet above the current water level, there is about 12 foot of vertical change that needs to be accommodated in a design. Alternative design ideas will be considered and may include an option for separating pedestrians and bicyclists at the bridge with a new pedestrian-only bridge over the river, which could potentially reduce the grade change to between about 7 to 8 feet.

In the context of the size of this site and the length of the Pier, an accessible path that accommodates 7, or possibly 12, foot of vertical change can be integrated into the full experience of the waterfront and be visually unified with the character of the site and Pier. With universal access as a foundational project goal, the sloping boardwalks and sidewalks become the access ways that everyone uses and not an appendage on the “real” part of the project.



The South Fishing Pier on Belle Isle afforded an opportunity to improve the ecological quality of the Detroit River through the creation of a 2.5 acre protected coastal wetland and shallow water nursery habitat.

Compelling Design:

Compelling Experiences

More important than making the grades work is designing an experience that leads, if not compels, the waterfront visitor toward the pier and related facilities. The waterfront should be much like a necklace—the path (as a fine chain of gold) with activity hubs (elegant jewels) spaced at suitable intervals to increase visual interest and lead the people from point to point.

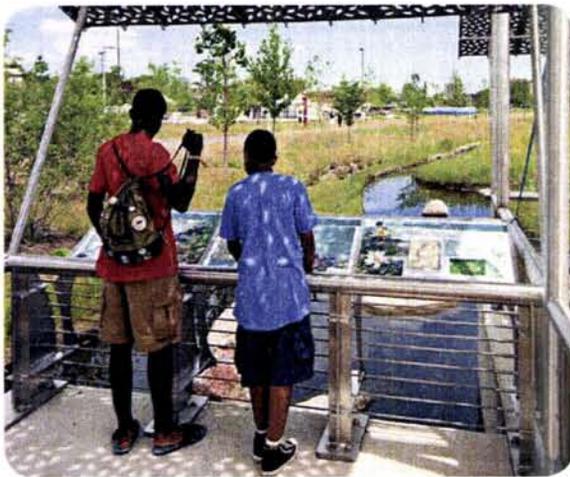
As illustrated in the sketches on the following pages, these activity hubs could include:

- A boardwalk node near the shoreline of the bay on the east side of the river that satisfies the urge to experience the bay after passing under the Grandview Parkway Bridge.
- Overlooks along the TART Trail on both sides of the river.

- Iconic design features that focus the experience on the water and become part of the wayfinding system.
- A sweeping path to the beach that offers a panoramic view of the bay and boats.
- Carefully crafted and elegant placemaking nodes or features on the Public Pier that insist the visitor experience them, culminating with an exclamation point at the north end of the Pier!
- A windswept boardwalk that engages the beach as it moves you west toward Clinch Park.



Alternative cross sections will be considered to accommodate heavy use of the TART.



Nodes and overlooks at Milliken State Park and Harbor provide access to and places of interpretive learning.

Exploring: Alternatives

For the purposes of illustrating a range of potential improvements to accessing the waterfront, we have explored several ideas and provided some conceptual diagrams. During the actual design process our team will complete a thorough analysis of the site and current non-motorized users and use patterns, which will help us gain a specific understanding of how access needs to function and provide us with a basis for exploring other alternatives.

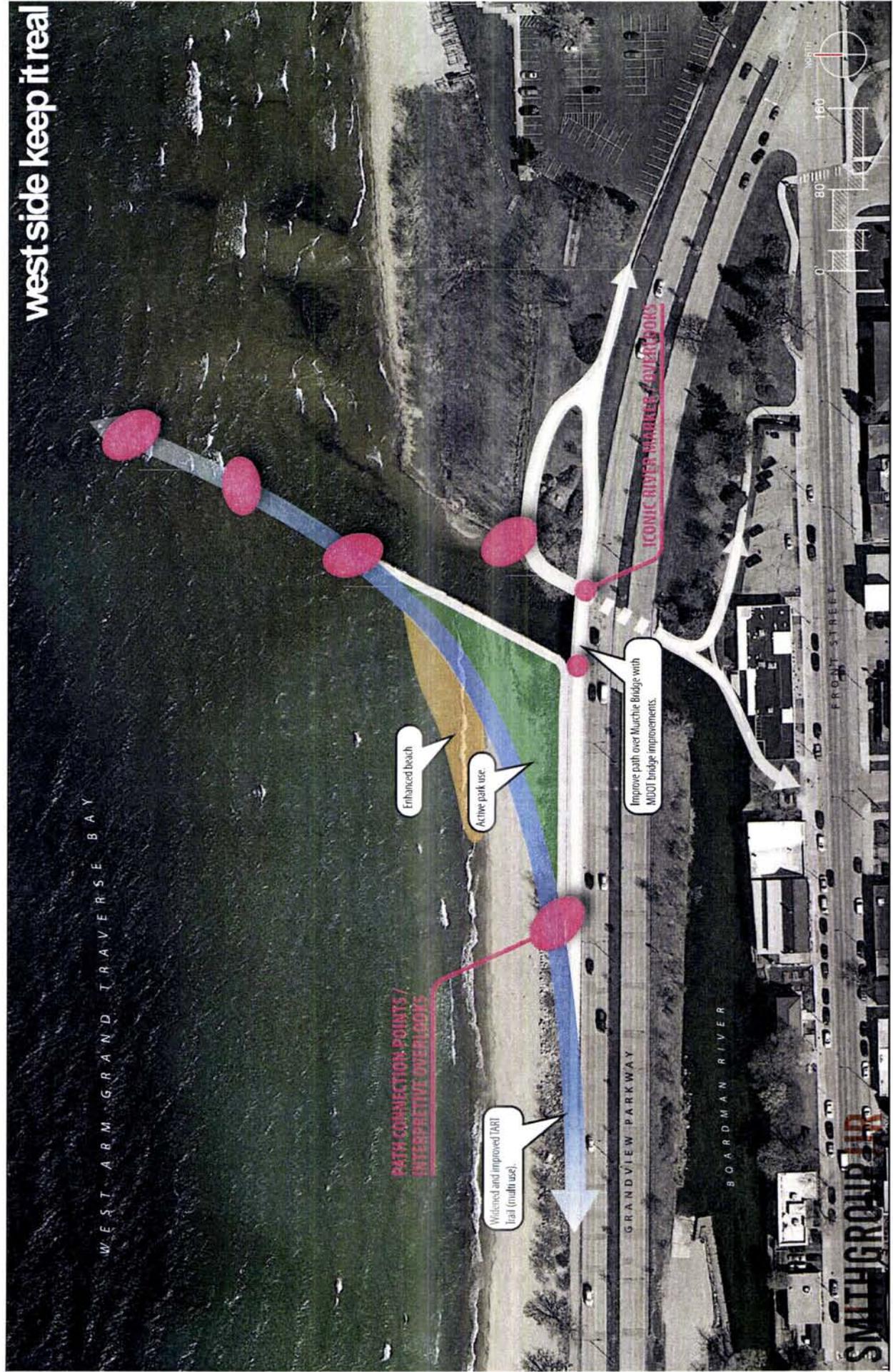
The three alternatives presented on the following pages offer an array of potential approaches, including:

- *West Side Keep it Real* – Straightforward design ideas to lead people to the Pier. This option improves the TART as a multi-use trail and creates an active, programmed park area at the base of the Pier to provide activity and energy to the area.
- *West Side Double-Double* – Recognizes the high levels of non-motorized traffic in this area and provides a pedestrian-focused crossing of the Boardman and pushes a smaller boardwalk out to the bay's edge on the east side of the river.
- *East Side Fly* – Places the primary Pier on the east side of the Boardman and dramatically pulls trail users from Clinch Park and downtown to experience the Pier.

Our team's proven ability to successfully overcome technical challenges (like the access grades) while designing energetic, thought provoking, and inspiring places is a hallmark of our work. We don't know the right answer yet, but are confident in our ability to create a compelling (and feasible!!) access to the waterfront in partnership with the community. We are committed to bringing you a wide range of creative solutions and working with you and the community to determine the approach that best fits Traverse City.

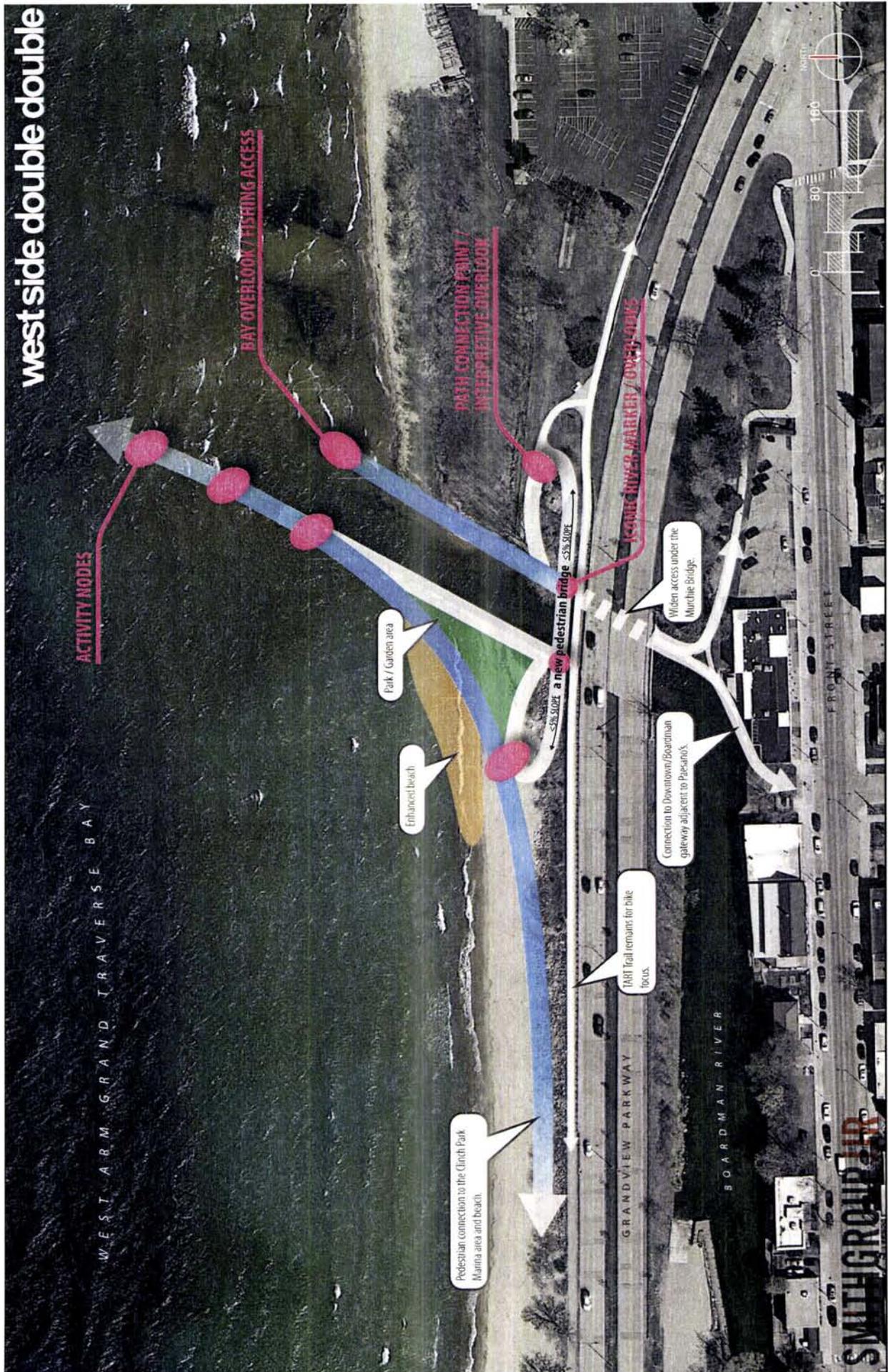
west side keep it real

WEST ARM GRAND TRAVERSE BAY



SMITHGROUPJJR

west side double double



ACTIVITY NODES

BAY OVERLOOK / FISHING ACCESS

PATH CONNECTION POINT / INTERPRETIVE OVERLOOK

BOARDMAN RIVER MARKER / OVERLOOKS

Park / Garden area

Enhanced beach

a new pedestrian bridge

TABT Trail remains for bike focus

Connection to Downtown/Boardman gateway adjacent to Paesanos

Widen access under the Murchie Bridge

WEST ARM GRAND TRAVERSE BAY

Pedestrian connection to the Clinch Park Marina area and beach

GRANDVIEW PARKWAY

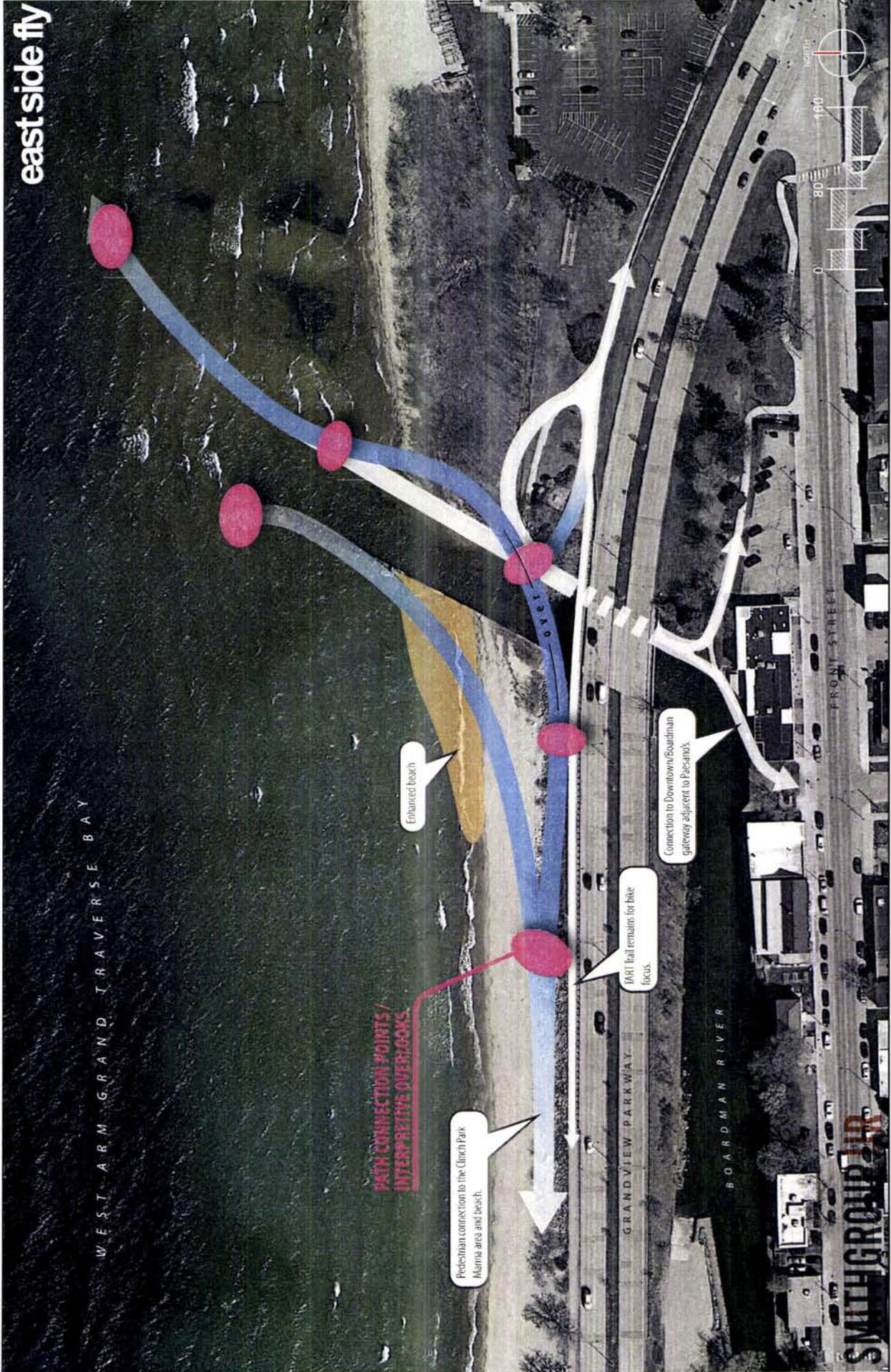
BOARDMAN RIVER

FRONT STREET

SMITHGROUP | JR

east side fly

WEST ARM GRAND TRAVERSE BAY



4 | COASTAL, WATERFRONT + HABITAT FISHERIES/

SmithGroupJJR's Team is composed of some of the country's foremost waterfront and coastal engineers with experience in master planning, design and implementation of coastal shoreline and erosion control protection and recreational boating and public facilities throughout the country.

Our project team includes engineers who served as contributing authors to the most recent edition of the American Society of Civil Engineers *Manuals and Reports on Engineering Practices No. 50, "Planning and Design Guidelines for Small Craft Harbors"*.

For the past 20 years, our team of coastal engineers have regularly presented at the annual *Docks and Marinas Conference*, organized through the University of Wisconsin-Madison Department of Engineering. Our staff have presented on many topics, including:

- Understanding wave action and designing breakwaters and wave attenuators.
- Planning, design and construction of marina structures.
- Hurricane protection for marinas and shore structures.
- Maintenance and improvements to marina dockage and facilities.

At the conclusion of this section, we have provided a detailed matrix illustrating the comprehensive experience of each team member in regards to the design and construction of piers on the Great Lakes as well as oceans, rivers and lakes. Although not an all-inclusive list, the *Pier Experience Matrix* clearly demonstrates the depth of experience in the design and engineering of waterfront spaces and places, including the coastal and hydraulic analysis of waterfront environments.

Coastal Modeling, Design Engineering + Construction

SmithGroupJJR coastal engineers are hands-on and in the field, conducting bathymetric surveys, existing condition assessments, numeric and physical modeling, and on-site construction administration.

We use a combination of numeric and physical modeling as a tool to help ensure the best possible outcome for our clients. This may include using numeric modeling

Coastal Modeling Experience

- Edgewater Marina Rehabilitation, Cleveland, OH
- North Coast Harbor, Cleveland, OH
- Clinch Marina, Traverse City, MI
- Kenosha Harbor Modeling Study, Kenosha, WI
- Southport Marina, Kenosha, WI
- Fisherman's Cove Marina, Bellingham, WA
- Euclid Waterfront Master Plan, Euclid, OH
- Sister Bay Marina and Shoreline, Sister Bay, WI
- Egg Harbor Marina Reconstruction, Egg Harbor, WI
- Geneva State Park Harbor, Geneva, OH
- Knife River Marina, Knife River, MN
- Grand Marais Marina, Grand Marais, MN
- Hoakalei Resort, Oahu, HI
- Barrier Island Conceptual Design, Seabrook, TX
- Navy Pier Rehabilitation, Chicago, IL
- Lakeshore State Park, Milwaukee, WI
- Milwaukee Art Museum Shoreline Stabilization and Lakefront Promenade, Milwaukee, WI
- Forest Park Shoreline Stabilization and Restoration, Lake Forest, IL
- Riverfront Redevelopment Phase I Design, Paducah, KY
- Clarksville Riverfront Planning and Design, Clarksville, TN
- Mississippi Riverfront Plan and Design, Muscatine, IA
- Reef Point Marina, Racine, WI
- Concordia University Lakefront Bluff Stabilization, Mequon, WI
- Rosewood Park, Highland Park, IL
- Silver Bay Harbor of Refuge, Silver Bay, MN
- Apostle Islands Shoreline Stabilization, National Park Service, Bayfield, WI

to identify options to improve the wave climate and sedimentation occurring at existing or proposed facilities. In other instances SmithGroupJJR has constructed a physical model of the planned improvements. The physical models have allowed SmithGroupJJR's designers and engineers to test concepts and develop optimized solutions that can help save client's money and provide optimal protection and structural integrity.

Our team of engineers work collaboratively and are integrated with our design team on developing cost-effective and long-lasting design solutions. This includes developing various conceptual designs, refining these designs, and providing on-site construction administration to ensure our designs get built to our standards.

CASE STUDIES

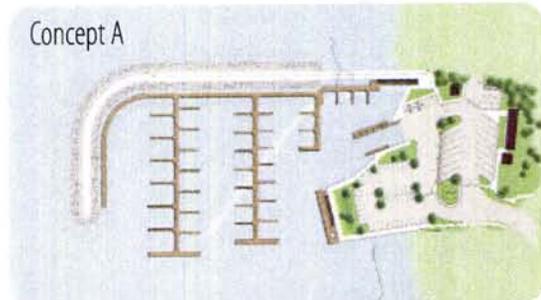
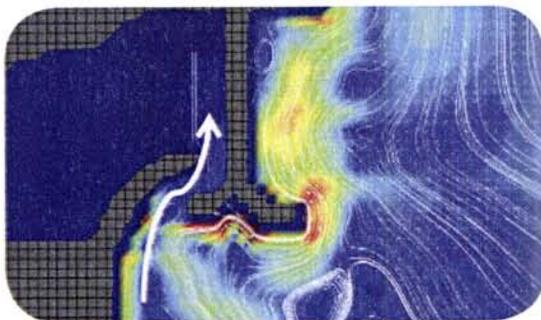
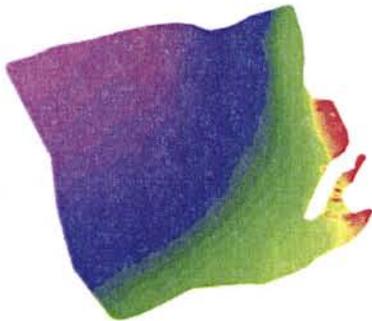
Written, Graphic and Pictorial Project Samples

SmithGroupJJR provides our clients with a variety of exhibits throughout the process, starting from initial concept and schematic design through to implementation. To do this, we provide our clients with diagrammatic graphics, computer and hand drawn renderings and cross sections, 3D models, wind and wave modeling graphics and videos (when appropriate), written reports, and ultimately, legible and precise construction documentation.

Egg Harbor/ Egg Harbor, WI

Wave and Wind Modeling

As part of the initial feasibility study, SmithGroupJJR conducted numerical wave and wind modeling of the existing harbor conditions. These models influenced the concept designs that were developed during the conceptual phase.



Concepts

SmithGroupJJR developed a series of concept plans that addressed the Village's primary concerns. Options ranged from rehabilitating the existing breakwater and boat slip arrangement in the current configuration, to developing an expanded, pedestrian-accessible breakwater that allowed the Village to provide additional marina boat slips, and improved shore protection structures with a publicly accessible feature.

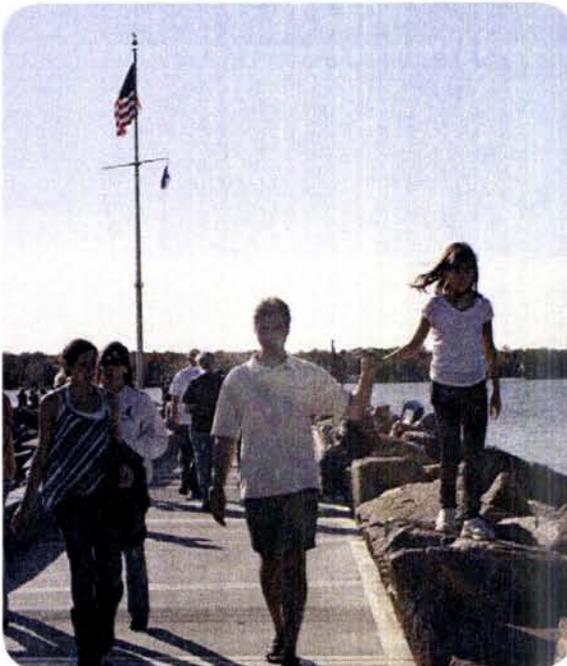
3D Modeling

To help the client visualize the final design, SmithGroupJJR built a 3D model of the proposed breakwater and marina. The 3D renderings helped lessen concerns from the client and the public that the new breakwater would impact their scenic sunset views. The models not only allowed the public to see the minimal impact on their views, but were also used as part of a public referendum to raise funds for construction of the final marina, and in state and federal grant applications.



Construction and Implementation

SmithGroupJJR engineers prepared the final construction documentation for the Egg Harbor Marina Rehabilitation project, and provided on-site construction administration throughout the construction process.



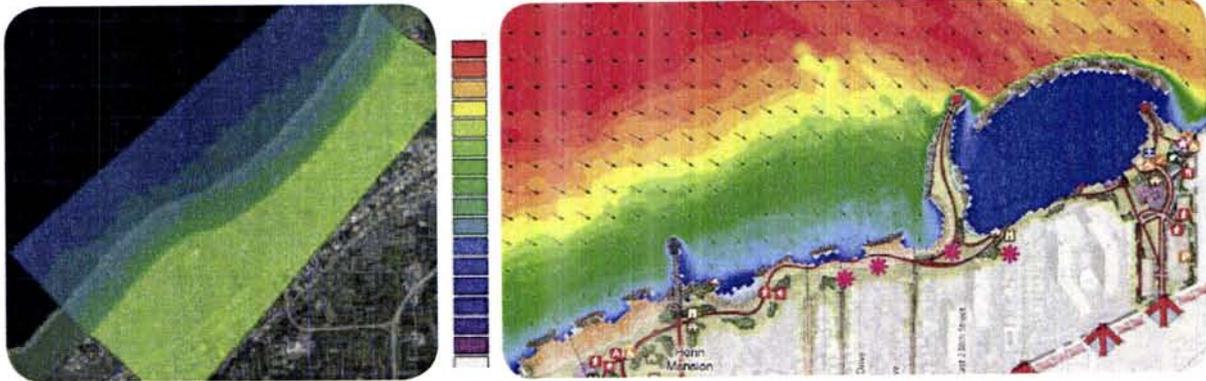
"I think the partnership with the hydraulic modeling lab in Ontario is a great relationship and absolutely tops. We flew up to Ontario to see the model and met the department head of coastal engineering of Queens University. We saw our model in use and could ask questions about our design, which was very helpful. SmithGroupJJR very clearly has their hands around all the technical issues. They were absolutely prepared for every question we could throw at them."

-Josh Vanlieshout, Village Administrator - Egg Harbor

Euclid Sim's Park Fishing Pier/Euclid, OH

Bathymetric Surveying and Modeling

For years the City of Euclid, Ohio has been looking for ways to improve their waterfront access to Lake Erie. As part of the Euclid Waterfront Improvement Plan, SmithGroupJJR conducted a shoreline site investigation, including bathymetric surveying, modeling, and field investigations to fully understand the environmental conditional along the lake front.



Field Investigation

To help advance the larger Waterfront Access and Improvement Plan, the team identified one location along the shoreline to complete a pilot project for the overall waterfront vision. The team proposed repurposing the substructure of an existing 150 foot storm sewer outfall and redesigning it as a publically accessible fishing pier.



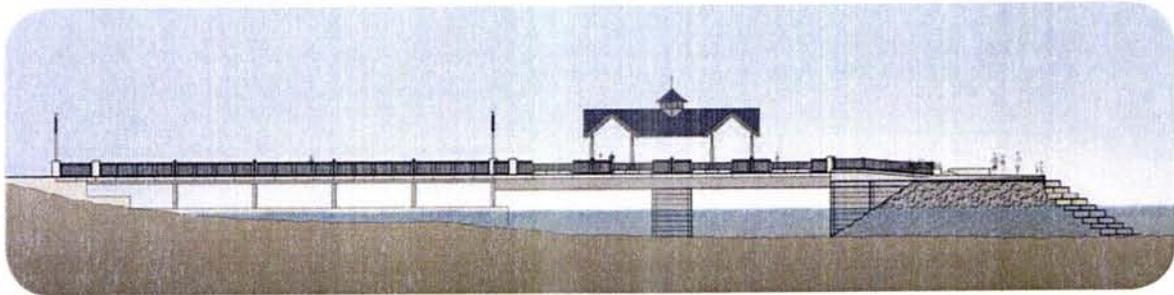
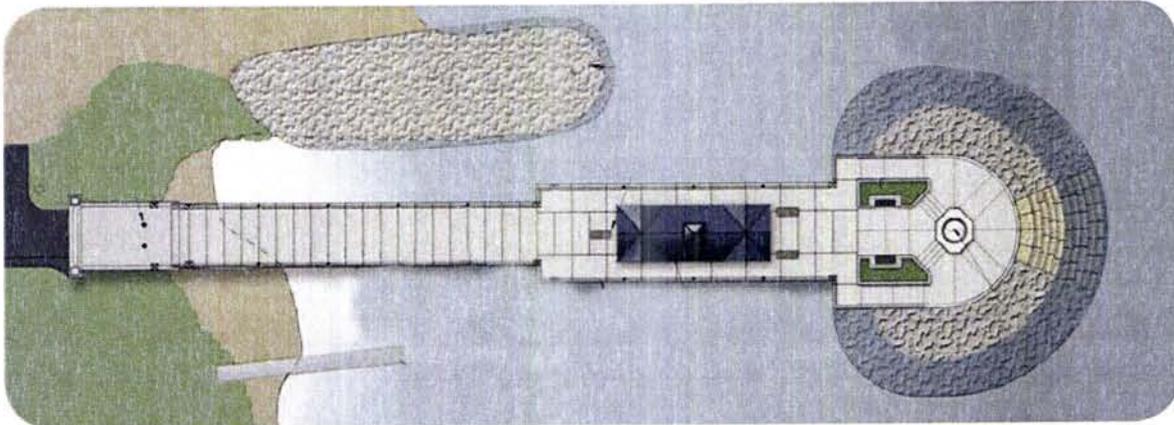
Before



Before

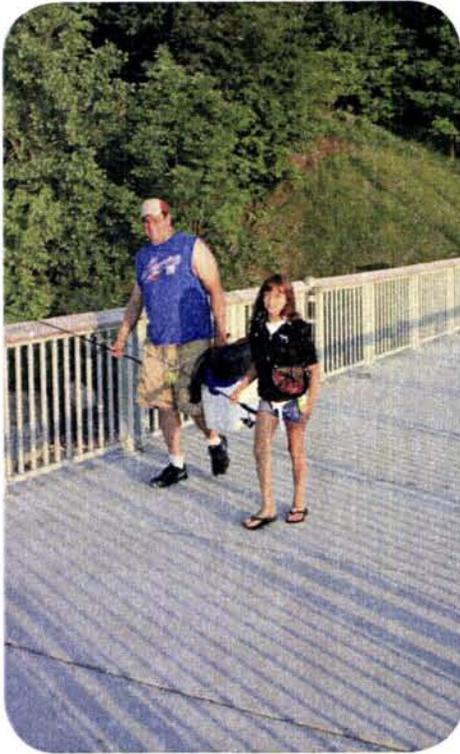
Concept Design and Design Development

SmithGroupJJR developed a series of conceptual level drawings to present to the City and to the public. These renderings were used to gain public support and as grant fundraising tools. The designs included providing ADA-accessible fishing access and railing design, shade structures, lighting, stepped access to the water (which also serves as an ice-breaker in winter weather), raised planters, benches, and a vertical-axis helical wind turbine.



Implementation and Final Construction

The Sims Park Fishing Pier was completed in 2013 and has since become a popular destination for area residents and fishing enthusiasts. The new fishing pier is also the location for the “Summer at Sims” Summer Event series. Events have included musical events, ski and water shows, and food festivals.



Lake Forest Shoreline Park/Lake Forest, IL

Lake Forest's shoreline at Lake Forest Beach was slowly disappearing through a combination of record-high lake levels, wave action, storm damage, and nearby man-made structures that cut off natural sand deposits. Consequently, the sandy beach at the bottom of the bluff was reduced to a thin line of gravel.

Using computer modeling, SmithGroupJJR's designs called for offshore breakwaters and onshore stone revetments. The beach was restored by adding sand along the shoreline, allowing natural wave action to create the desired shoreline pattern.

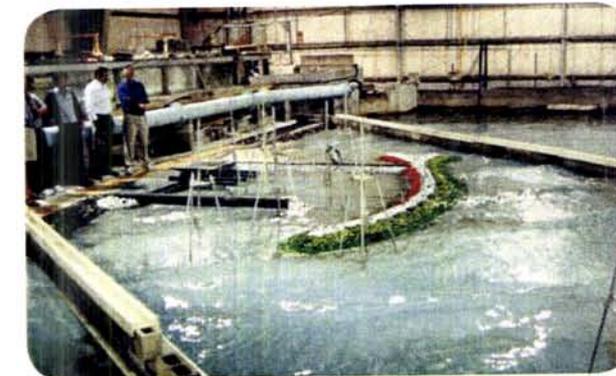
SmithGroupJJR worked with the entire team, including its coastal engineers to conduct hydraulic model testing using a 100-foot by 100-foot wave tank with lake bottom topography and shoreline contours that replicated Lake Forest conditions. The master plan also added several amenities to the renewed lakeshore, including a protected boat basin and launch area, beach house and landscape lawn area, timber walkways and overlook decks, and a popular waterfront promenade.



Bender Park/Oak Creek, WI

SmithGroupJJR provided master planning, environmental services, and final engineering for the development of a 302-acre parcel of land along 6,000 feet of Lake Michigan shoreline. The project included active and passive recreational facilities, a boat launch harbor, and fishing piers. Facilities included softball diamonds, soccer fields, picnic shelters, and play equipment, as well as walking and bicycle paths, nature boardwalks, and bioengineered shoreline protection.

The first phase of the project stabilized the bluff and protected the shoreline from further erosion. The second phase included a small boat basin, dockage, launch ramp, promenade, overlook structure, fishing areas, park building, parking, roads, landscaping, and lighting. SmithGroupJJR was responsible for all construction documents and administration on this award-winning project.



UW-Madison Alumni Pier/Madison, WI

The Memorial Union Pier and Terrace Shoreline on Lake Mendota is part of UW-Madison's Memorial Student Union. SmithGroupJJR was hired to help restore the failing stone shoreline along the terrace and to design, engineer, and implement a new transient marina for area boaters.

The new pier was designed to accommodate both a fixed pier and floating dockage for transient boats and paddlecraft. The fixed pier is designed to remain in place year-round and is engineered to withstand the Lake Mendota fetch, the associated wave climate, as well as the harsh wind and ice conditions of the southern Lake Mendota shoreline.

The adjacent floating dockage is put in place seasonally and provides boat slips for 15 boat and 8 paddle crafts (including canoes and kayaks). The floating dockage was designed to withstand the variable wave climate of the lake.

All design, engineering, coastal environment modeling and analysis and construction documents were prepared by SmithGroupJJR.



Milwaukee Lakeshore State Park/Milwaukee, WI



Lakeshore State Park is a \$14 million, 17-acre man-made island that is Wisconsin's first urban state park. SmithGroupJJR led the project team and provided planning, design, modeling, coastal and civil engineering, permitting and construction administration for the project. To ensure that the shore protection structures would withstand the extreme Lake Michigan wave and ice environment, SmithGroupJJR built a physical model of the proposed island and the breakwater structures.

Site elements include a large and small watercraft basin, transient boat dockage, small boat beach, fishing access, interpretive features, naturalistic open space, bioengineered shoreline design, native landscape, a signature pedestrian bridge, and over two mile of accessible waterfront and shoreline pathways.

South Pier Development/Sheboygan, WI

SmithGroupJJR has had a long-standing relationship with the City of Sheboygan that has resulted in a number of planning and design efforts.

Harbor Centre Marina, now a showcase projects for Sheboygan, transformed an area historically used as a fishing, shipping, and industrial facility into a vibrant and active, public waterfront destination.

SmithGroupJJR provided planning, design, and engineering for the implementation of the harbor, 440-slip marina, and public waterfront promenade. The promenade connects users to the Harbor Centre Marina, the Sheboygan Youth Sailing School, the Sheboygan Yacht Club, and public parks and beaches, parking, and park trails and landscape design.



Silver Bay Harbor of Refuge/Silver Bay, MN



SmithGroupJJR prepared plans and specifications for construction of the Silver Bay Harbor of Refuge along the north shore of Lake Superior. The engineering effort included client workshops, hydrographic and topographic surveys, environmental assessment, hydraulic modeling, and geotechnical investigation. The site design and construction documentation included breakwater, marina dockage, launch ramp, parking, service building layouts, fuel system, and sanitary pumpout. The marina facility has 160 boat slips and a building with restrooms, showers, laundry facilities, and concessions. SmithGroupJJR's solutions optimized performance and minimized cost all within an appropriate response to community and site context.

Concordia University Lakeshore Enhancements/Mequon, WI

SmithGroupJJR provided master planning, design, and engineering that has improved and enhanced the university's lakefront property. The plans included implementing a massive shoreline erosion and slope stabilization effort for 2,700 feet of Lake Michigan shoreline. The project also included the development of coastal and perched wetlands, environmental habitat enhancement areas, recreational waterfront improvements, and restoration of the bluff with native vegetation. The new shoreline, beaches, and trails are fully accessible to the public, and provide the university with an outdoor learning laboratory for environmental science programs.



TEAM MEMBERS/

Margaret Boshek, PE

Project Coastal Engineer



Education

Master of Science, Coastal & Marine Engineering and Management, Delft University of Technology, The Netherlands, 2009

Universitat Politècnica de Catalunya, Spain, 2009;
Norwegian University of Science and Technology, Norway 2009

Bachelor of Science, Ocean Engineering, Florida Institute of Technology, Melbourne, Florida, 2003

Registrations

Professional Engineer:
British Columbia, Wisconsin

Professional Affiliations

American Society of Civil Engineers (ASCE)

PIANC

Margaret is a coastal engineer with extensive project experience throughout the United States and internationally working on a myriad of coastal and marine projects. She worked for four years as a corrosion specialist, marine structural engineer, and waterfront and marine designer before obtaining her masters in Coastal and Marine Engineering and Management from TUDelft, the world's leading university for coastal studies, as an Erasmus Mundus full scholarship awardee. She spent one and half years working in The Netherlands as a hydraulic engineer focusing on coastal and river flood defense and dike design including geotechnical analysis and modeling. During this time she worked on the team designing the doors for the new Panama Locks. Margaret spent four years as a lead coastal engineer and project manager in Canada specializing in the planning and design of port terminals, commercial and recreational marinas, and ferry terminals, numerical modeling of coastal dynamics, mooring analysis, floating structure response, waterfront design and construction engineering, specification preparation, and risk analysis. She also has experience in hydrodynamic analysis, beach morphology, wave attenuation and harbor tranquility, physical modeling, construction oversight, field inspections, and oversight of client and public workshops and presentations. Her international experience and project diversity make her well-suited for all aspects of coastal project execution from concept through construction.

- Kenosha Harbor and Southport Marina Sedimentation Study, Kenosha, Wisconsin
- Amador Marina, Panama City, Panama
- Buenaventura Marina, Panama City, Panama
- Fisherman's Cove Marina, Bellingham, Washington
- Public Works & Services Canada Feasibility Study: Marine Infrastructure in Nunavut
- Port Metro Vancouver Roberts Bank Terminal 2, Delta, British Columbia
- Georgia Pacific Log Pond Shoreline Restoration, Bellingham, Washington
- Marina Pez Vela, Quepos, Costa Rica
- Department of Fisheries and Oceans Condition Assessment, Batchawana Bay, Ontario
- McNally Almiq JV Pangnirtung Harbor Development, Pangnirtung, Nunavut
- Catalyst Paper Co. Chip Barge Mooring Design, Powell River, British Columbia
- British Columbia Ferries Tsawwassen Ferry Terminal Propeller Scour Maintenance, Tsawwassen, British Columbia
- Defense Construction Canada Nanisivik Naval Facility, Nanisivik, Nunavut
- Ledcor CMI Ltd. Floating Wave Attenuator Design, Vancouver, British Columbia
- Albion Sands TSRU Floating Pipeline Mooring, Muskeg River Mine, Alberta
- Department of Fisheries and Oceans Condition Assessment, Batchawana Bay, Ontario
- Chevron Canada Limited Hatch Point Wharf Repairs, Hatch Point, British Columbia
- Westshore Terminals Limited Partnership Metocean Study & Vessel Motion Study, Delta, British Columbia
- Centrica LNG Port Hurricane Metocean Study, Boca Chica, Dominican Republic
- Kesho Inc. Lulu Island Dyke Design – New Westminster, British Columbia
- Panama Canal Expansion, Third Locks – Panama

Bill Brose, PE

Waterfront Engineering Resource



Bill has 25 years experience as one of SmithGroupJJR's most accomplished waterfront engineers. His involvement begins at the conceptual design level and follows through to construction completion. Bill is experienced in feasibility studies, client and public participation workshops, data acquisition and analysis, underwater data gathering and inspections, construction materials investigations, waterfront design and construction engineering, infrastructure design, and construction plan and specification preparation. He also offers proven expertise in permitting and regulatory agencies, client coordination, and public involvement. Mr. Brose is a co-author of the update for the *ASCE Manual of Engineering Practice 50, Planning and Design Guidelines for Small Craft Harbors*.

Education

Bachelor of Science Civil Engineering, University of Wisconsin-Madison, 1984

Post Graduate Studies-Coastal Engineering, University of Wisconsin-Madison, 1988-89

Registrations

Professional Engineer: Wisconsin, Illinois, Minnesota, New York, Pennsylvania, Ohio, Hawaii, Colorado, Kentucky, Tennessee, Arizona, South Dakota, Idaho

Professional Affiliations

American Society of Civil Engineers

- Apostle Islands National Lakeshore, National Park Service, Bayfield, Wisconsin
- Caesar Creek Marina Development, Warren County, Ohio
- Southport Marina, Kenosha, Wisconsin
- Agate Bay Marina, Two Harbors, Minnesota
- Algoma Harbor, Algoma, Wisconsin
- Ashland Kiyi Research Vessel Dockage, Ashland, Wisconsin
- Atwater Park Master Plan, Shorewood, Wisconsin
- Bayfield Apostle Islands Marina Improvements, Bayfield, Wisconsin
- Bayport Condition Assessment, Bayport, Minnesota
- Belle Harbor and Pershing Boat Launch Racine, Racine, Wisconsin
- Bender Park Harbor Shoreline and Marina, Oak Creek, Wisconsin
- Caesar Creek Marina, Warren County, Ohio
- Chatcolet Marina, Plummer, Idaho
- Chatfield Reservoir Relocation, Littleton, Colorado
- Clarksville Liberty Park and Marina, Clarksville, Tennessee
- Duncan L. Clinch Marina, Traverse City, Wisconsin
- Clinton Marina, Clinton, Iowa
- Concordia Center for Environmental Stewardship, Mequon, Wisconsin
- Concordia University Wisconsin Master Plan Update, Mequon, Wisconsin
- Crab Orchard Refuge Marina Evaluation, Marion County, Illinois
- Downtown Milwaukee Lakefront Development, Milwaukee, Wisconsin
- Edgewater Marina Rehab, Cleveland, Ohio
- Egg Harbor Marina Rehabilitation, Egg Harbor, Wisconsin
- Euclid Harbor Feasibility Study, Euclid, Ohio
- Fisherman's Cove Working Waterfront, Bellingham, Washington
- Geneva Sand Bypass Investigation, Geneva, Ohio
- Gills Rock Harbor of Refuge Marian Study, Gills Rock, Wisconsin

Jack Cox, PE

Coastal Engineering Resource



Education

Master of Science Engineering,
Purdue University, 1973

Bachelor of Science, Engineering
Science, Purdue University, 1973

Post Graduate Coursework,
University of Chicago, 1974

Doctoral Studies, University of
Delaware, 1977 (Davis Fellow)

Registrations

Professional Engineer:
Maryland, South Carolina, New
York, Washington, Florida,
Wisconsin, Indiana, New Jersey,
Illinois, Missouri, Delaware,
Ohio, Rhode Island

Associations

Permanent International Associa-
tion of Navigation Congresses US
Commissioner for Recreational
Boating

Academy of Coastal, Ocean,
Port and Navigation Engineers
Inaugural Diplomate
Engineering

Association of Coastal Engineers
– Board of Directors

Tsunami Advisory Board at the
University of Washington

Jack is internationally recognized in waterfront development, including coastal dynamics, harbor tranquility, breakwaters, fixed and floating marine structures, shore protection, port planning, marina design, and risk analysis. Jack is the inaugural class Diplomat for Coastal, Port and Navigation Engineering, with the Academy of Coastal, Ocean, Port, and Navigation Engineers. He is a member of the Tsunami Advisory Board at the University of Washington, and is the Vice Chairman of the Permanent International Association of Navigation Congresses Recreational Navigation Commission. He has authored over 40 coastal publications and is a contributing author of the update for the *ASCE Manual of Engineering Practice 50, Planning and Design Guidelines for Small Craft Harbors*. He is a lecturer at the University of Wisconsin on marina design and shoreline protection engineering. In 2003, Jack was awarded the Distinguished Lecturer Award by the University of Wisconsin for his instructions in the areas of coastal and hydraulic engineering.

- Makronisos Marina, Ayia Napa, Cyprus
- Algoma Harbor Master Plan, Algoma, Wisconsin
- Bayview Harbor Marina Recommendations, Ellison Bay, Wisconsin
- Chatfield Marina Rehabilitation, Littleton, Colorado
- Feasibility Study and Waterfront Improvements Plan, Euclid, Ohio
- Fisherman's Cove Working Waterfront, Bellingham, Washington
- Ocean Reef Marina, Panama City, Panama
- La Pointe Harbor Master Plan, Town of La Pointe, Wisconsin
- Paducah Riverfront Marina and Waterfront Improvements, Paducah, Kentucky
- Vacamonte Beach and Marina, Panama
- Washburn Marina Design and Engineering Study, Washburn, Wisconsin
- Vacamonte Port Feasibility, Panama
- Wilmette Lakefront Master Plan, Wilmette, Illinois
- Caesar Creek Marina Development, Warren County, Ohio
- Geneva State Park Sediment By-Pass and Transient Docks, Geneva, Ohio
- Port of La Crosse Harbor and Waterfront Plan, La Crosse, Wisconsin
- Marina Pez Vela, Quepos, Costa Rica*
- Ft. Pierce Marina Harbor Breakwater Design, Ft. Pierce Florida*
- Middle Bass Island Marina Development, Lake Erie, Ohio*
- Tianjin Marina, China*
- Vista Mar Marina, San Carlos, Panama*
- Imperial Pacific Beach Marina, Jaco Beach, Costa Rica*
- Harbor of Americas, Bocas del Toro, Panama*
- Playa Celeste Marina and Village, Guanacaste, Costa Rica*

Douglas Denison

Fisheries and Habitat Restoration Resource



Education

Masters of Science in Water Resource Science, University of Michigan, 1977

Bachelor of Science in Aquatic Biology, Eastern Michigan University, 1974

Certifications

Certified in Project-Level Quality Documentation (2010), U.S. Environmental Protection Agency, Great Lakes National Program Office and Office of Water

Professional Affiliations

Huron Erie Corridor Initiative

National Wildlife Federation

Environmental Fund for Michigan - Board of Directors (1999-2001)

EarthShare of Michigan Board of Directors (2001-2002)

Great Lakes Fishery Trust Scientific Advisory Team

Michigan Lakes and Streams Association (1976-2000)

Society of Wetland Scientists

Michigan Society of Planning Officials (1989-2000)

American Water Resource Association

Michigan Water Environment Association

Water Environment Federation

Doug was a leading member of SmithGroupJJR's natural resources practice for over 20 years providing scientific leadership and project management in environmental planning, analysis and mitigation. As the senior environmental scientist, he led a variety of projects including Environmental Impact Statements, endangered species habitat evaluation, water resource analyses, mitigation design and construction oversight, public education, aquatic ecology evaluations, preparing stormwater management plans, environmental studies, NEPA compliance reports, water quality investigations, and agency coordination. Doug is currently working with SmithGroupJJR in a capacity as a natural resource expert emeritus. He is still a leading Great Lakes scientist and has successfully written numerous grants to aid in project implementation. In addition, Doug has years of experience working in the Boardman River Watershed System on such projects as the Boardman River Environmental Framework Study and an Environmental Assessment and an Environmental Impact Statement for MDOT for an east-west connector over the Boardman River.

- Belle Isle Park Deep-Water Habitat Fishing Piers, Detroit, Michigan
- Belle Isle/Detroit River Sturgeon Habitat Restoration, Detroit, Michigan
- Caddell Drain Erosion Control Aquatic Habitat Enhancement and Stabilization, Farmington Hills, Michigan
- Ellias Cove Shoreline Stabilization and River Habitat Restoration, Trenton, Michigan
- Fellows Creek Floodplain and Habitat Restoration, Canton Township, Michigan
- Gilkey Creek Relocation and Restoration, Streambank Stabilization, and Wetland Enhancements, Flint, Michigan
- Johnson Creek Hydrologic Evaluation, Streambank Stabilization Design and Implementation, and Aquatic Habitat Creation, Salem Township, Michigan
- Johnson Creek Headwaters Protection Program and Critical Habitat Assessment, Washtenaw County, Michigan
- BASF Point Hennepin Master Plan and Design Management Recommendations for Habitat Creation, Wyandotte, Michigan
- General Motors, Coldwater-Inland Fisher Guide Division Report of Habitat Restoration and Wetland Mitigation, Flint, Michigan
- Seven Lakes of Northville Cold Water Stream Restoration and Trout Habitat Creation, Northville, Michigan
- St. Clair River Delta Fish Spawning Habitat, Port Huron, Michigan
- Consumers Power Quantitative Analysis and Research on the Impact of Great Lakes Fisheries Resulting from Power Plant Operations, Port Sheldon, Michigan

SMITHGROUPJJR TEAM | PIER EXPERIENCE MATRIX

| Name of Pier | Body of Water | Role | Brief Description | Length of Structure | Project Cost |
|---|---------------------------------|--|---|----------------------|-----------------------|
| PAT DOHER, PE, LEED AP Navy Pier Revitalization | Lake Michigan | Project Engineer | Project restoration efforts included a new north dock pier, new north and south overlook platforms, stone revetment stabilization in depths up to 30 feet; foundation underpinning for the historic Head House; pump station; paving and repair renovation work. | 3,300 feet | \$21 Million |
| Detroit RiverWalk | Detroit River | Project Engineer/Project Manager | Project leader and engineer of record for 3 miles of public promenade along the Detroit River. Led the design and technical studies including survey, bathymetric survey, environmental studies, geotechnical investigations and river edge stabilization. Established basis of design for the riverine coastal environment. Managed the establishment of the program, basis of design, alternatives, design development, construction documents, public facilitation and administered construction for over \$25 million of public, universal access, urban connection improvements along the Detroit River. | 3.5 miles | \$27 Million |
| Omni Talon at Detroit RiverWalk - a portion of the Detroit RiverWalk | Detroit River | Project Engineer/Project Manager | Multi-purpose dock approximately 1,000 feet long by 25 feet wide. Pier allowed public access and program space including separate non-motorized path and pedestrian walkway. Responsibilities included supervision of underwater dive inspection, coastal analysis including ice force development due to floating and moving ice and debris, alternatives and development of construction documents as well as construction administration. | 890 feet | \$3.5 Million |
| Fishing Piers at Gabriel Richard Park - a portion of the Detroit RiverWalk | Detroit River | Project Engineer/Project Manager | Two fishing piers at each end of the RiverWalk at Gabriel Richard Park along the Detroit River. Developed concepts, prepared grant applications, secured grant and developed construction documents and administered construction for two universal access fishing piers. | 175 feet 104 feet | \$250,000 |
| Elizabeth Park Fishing Pier | Detroit River | Project Engineer | Universal access public pier providing fishing and passive recreation opportunities in Trenton, Michigan. | 1,700 feet | \$2.2 Million |
| Elmwood Township | Grand Traverse Bay | Project Engineer | Worked with Gourde-Fraser to plan and design retrofitting an existing breakwater to allow for public access and fishing. | 500 feet | TBD |
| South Haven Marina Park | Lake Michigan/Black River Mouth | Project Engineer | This public pier and public space was constructed of timber, with wood and metal rails with lights. The structure was designed to attenuate waves and provide temporary mooring for large ships. | 400 feet | \$600,000 (pier only) |
| Milliken State Park and Harbor | Detroit River | Project Engineer | Repair and reconstruction of river edge stabilization including the analysis of wave action, ice forces and boat docking along the Detroit River. This was an emergency repair and reconstruction of a structural dock along the State Park. | 100 feet | \$1,000,000 |
| Dime Pier Fireworks Dock at Navy Pier | Chicago Harbor (Lake Michigan) | Project Engineer/Project Manager | Coastal analysis and design of a dock structure south of Navy Pier for public access, boat docking and fireworks staging area for the historic pier between Navy Pier and the Locks at the Chicago River. Responsibilities included underwater dive inspection, basis of design, management of coastal analysis and establishing alternatives. | 1,800 feet | \$2.5 Million |
| BOB DOYLE, ASLA South Haven Marina Park | Lake Michigan/Black River Mouth | Lead Project Landscape Architect/Project Manager | This public pier and meeting place was constructed of timber, with wood and metal rails with lights. The structure was designed to attenuate waves and provide temporary mooring for large ships. | 400 feet | \$600,000 (pier only) |
| Milliken State Park and Harbor | Detroit River | Lead Project Landscape Architect/Project Manager | a public fishing pier and promenade, built over existing sheet pile wall and pile-supported structure. | 1,000 feet | \$1,000,000 |
| Elmwood Township | Grand Traverse Bay | Landscape Architect | Worked with Gourde-Fraser to plan and design retrofitting an existing breakwater to allow for public access and fishing. | 500 feet | TBD |
| Port Huron Blue Water River Walk | St. Clair River | Lead Project Landscape Architect/Principal-in-Charge | Lead Project Landscape Architect/Project Manager | 225 feet | TBD |
| Fishtown | Lake Michigan | Lead Project Landscape Architect | Master Planning for restoration of commercial fishing and retail public pier | 250 feet | TBD |
| Nautical Miller/Blossom Heath Park | Lake St. Clair | Lead Project Landscape Architect | Waterfront park design plans that included improvements to a popular fishing pier extending into Lake St. Clair | 550 feet | TBD |
| MARGARET BOSHEK, PE Sims RiverWalk Park | Lake Erie (Lucid, Ohio) | Project Coastal Engineer | Waterfront development extending from Sims Fishing Pier to a future marina site to the east. Development will stabilize steep erodible bluffs while creating a recreational beach and marine habitat. Coastal dynamics include longshore sediment movement and headland beaches. | 2,500 feet | \$20 Million |
| Marina Pez Vela | Pacific Ocean | Lead Coastal Engineer | Mega-yacht marina facility utilizing cellular cofferdam structures to provide access to marina dockage and deep water fishing opportunities. Structure crest was outfitted with yachting storage lockers and multilevel viewing platforms while protecting the marina basin from high swell waves. | 1,700 total feet | \$30 Million |

| Name of Pier | Body of Water | Role | Brief Description | Length of Structure | Project Cost |
|--|--------------------------------|--|--|--|----------------|
| Wild Rose Marina Access Pier | Lake Shuswap, British Columbia | Project Manager | Pile-supported access pier designed to provide access to a floating recreational marina subjected to high waves and ice effects. The pier was designed to allow for universal access and is therefore ADA compliant and detailed for all-weather use. | 300 feet | \$2.5 Million |
| Fort Pierce Marina Artificial Islands | Indian River, Florida | Coastal Engineer | Redesign of a marina destroyed by hurricane effects which included high surge and strong currents. Design included the implementation of artificial islands to divert sediment around a newly dredged marina basin. Islands provide habitat as well as recreational opportunities to a variety of users. | 1,500 feet | \$20 Million |
| Paris Landing Marina Breakwaters | Paris Landing, TN | Principal Coastal Engineer | Independently designed and engineered a protective set of breakwaters to mitigate wake waves plaguing a small craft marina from barge traffic on the Tennessee River. | 200 feet | \$1 Million |
| Hatch Point Terminal Re-design | Hatch Point, BC | Project Coastal Engineer | Engineered a re-design of a docking terminal to increase moorage capacity to meet market demands. Redesign included selection of materials to provide extended life to the structure and saline water wear resistance. | 500 feet | \$2.5 Million |
| Batchawana Fishing Dock | Batchawana Bay, ON | Project Coastal Engineer | Studied environmental loading on fishing pier including ice thermal forces and observed ice jacking. Made recommendations for retrofits that could be applied to the existing structure as well as a planned expansion. | 200 feet | \$500,000 |
| JACK COX, PE | | | | | |
| Sims RiverWalk Park | Lake Erie (Euclid, Ohio) | Senior Coastal Engineer/Civil Engineer | Waterfront development extending from Sims Fishing Pier to a future marina site to the east. Development will stabilize steep erodible bluffs while creating a recreational beach and marine habitat. Coastal dynamics include longshore sediment movement and headland beaches. | 750 feet | \$20 Million |
| Apostle Islands Excursion Ferry Dock | Lake Superior | Senior Coastal Engineer/Civil Engineer | Design of bin wall type access pier for ice push and loading. | 300 feet | \$1 Million |
| Hammond Indiana Marina | Lake Michigan | Senior Coastal Engineer/Civil Engineer | Designed a marina facility including dockage for 1,100 boats, utilities, and an innovative tandem breakwater system and conversion engineering of a retired historic cruise ship into a marina pavilion and festival center. | 2,000 feet | \$8 Million |
| Middle Bass Island Marina | Lake Erie | Senior Coastal Engineer/Civil Engineer | Design of new harbor infrastructure and breakwaters to emulate habitats. | Basin created 20 acres + 800 foot jetties | \$9 Million |
| Fort Pierce Marina Artificial Islands | Indian River, Florida | Senior Coastal Engineer/Civil Engineer | Redesign of a marina destroyed by hurricane effects which included high surge and strong currents. Design included the implementation of artificial islands to divert sediment around a newly dredged marina basin. Islands provide habitat as well as recreational opportunities to a variety of users. | 1,500 feet | \$20 Million |
| Elliott Bay Marina | Puget Sound (Seattle, WA) | Senior Coastal Engineer/Civil Engineer | Design of supplemental island breakwaters to control near-shore wave rejections while allowing fish migration. | 1,000 feet | \$12.7 Million |
| UW-Madison Memorial Union Pier | Lake Mendota | Principal Engineer | Development of a combination of fixed and floating pier system for the Memorial Union Terrace. The pier serves as a recreational waterfront hangout for students and residents, and provides additional access to Lake Mendota. | 85 feet | \$7 Million |
| BILL BROSE, PE | | | | | |
| Lakeshore State Park | Lake Michigan | Project Manager | The Lakeshore State Park in downtown Milwaukee includes over 2 miles of pedestrian accessible shoreline paths along Lake Michigan. These paths provide shore protection from wave, wind and ice damage and provide public access to the waterfront. | Over 2 Miles | \$17 Million |
| Clarksville Fishing Pond and Marina | Cumberland River | Principal | Created a fully accessible fishing pond and fishing boardwalks/piers as part of a 130 acre recreational park. Boardwalk and fishing pier access total over 1700 linear feet. | 1,700 linear feet | \$34 Million |
| Egg Harbor Marina and Breakwater | Lake Michigan | Principal | Created a 750 foot long publicly accessible breakwater that serves as a fishing pier, sunset lookout, and provides overall community access to the lake. | 750 linear feet | \$7 Million |
| Euclid Fishing Pier | Lake Erie | Principal | Repurposed a former drainage sewer substructure into a ADA-accessible fishing pier. | 250 feet | \$1.1 Million |
| Bender Park Breakwater Pier | Lake Michigan | Principal | As part of the boat launch and beach improvements, SmithGroupJJR provided created a 200 foot publicly accessible pier as part of the breakwater protection. | 200 feet | \$17 Million |
| Lake Forest Beach Park and Shoreline | Lake Michigan | Project Manager | As part of the shoreline restoration along Lake Forest's waterfront, SmithGroupJJR designed a series of breakwaters to reclaim the beach and protect against erosion. The main breakwater along the shore also serves as a publicly accessible waterfront access and pier for the community. | 110 feet | \$8 Million |
| UW-Madison Memorial Union Pier | Lake Mendota | Principal Engineer | Development of a combination of fixed and floating pier system for the Memorial Union Terrace. The pier serves as a recreational waterfront hangout for students and residents, and provides additional access to Lake Mendota. | 85 feet | \$7 Million |

| Name of Pier | Body of Water | Role | Brief Description | Length of Structure | Project Cost |
|--|--|--|---|--|----------------|
| NEAL BILLETTEAUX, ASLA, LEED AP BD+C Frankfort-Eberta Beach to Beach Trailway | Betsie Lake-outlet to Lake Michigan | Landscape Architect | Prepared a Master Plan for the Frankfort-Eberta Beach-to-Beach Trailway, a four-mile multi-use recreational trail around Betsie Lake that connects two Lake Michigan beaches. The universally accessible trailway provides a recreational attraction to enhance local economies and improved access to Betsie Lake's fishing resources. Portions of the trailway will be located on abandoned State-owned railbeds, along waterfronts, through parks, and near shops, restaurants, marinas, fishing areas, boat launches and historical sites and structures. A phased program will accommodate trailway implementation, and future improvements including restrooms, fishing piers, canoe launches, information centers and streetscape elements. | n/a | \$9 Million |
| Portage Lakefront and Riverwalk | Along Burns Waterway-outlet to Lake Michigan | Environmental Designer/Landscape Architect | Development of a new lakefront park at the confluence of Lake Michigan and Burns Waterway in Portage, MI. The sixty-acre park, on a former industrial brownfield site now within the Indiana Dunes National Lakeshore, was cooperatively developed between the City of Portage and National Park Service and is Portage's first public park on Lake Michigan. The park is a model of sustainable development and includes natural areas and trails, public beach areas, a riverwalk along Burns Waterway, numerous interpretive opportunities and a LEED gold-rated public pavilion with concessions, toilets and public classroom/meeting space. | 85 foot fishing pier 1,120 foot breakwater pier | |
| EMILY MCKINNON, PE, LEED AP | | | | | |
| Wyandotte Transient Marina | Detroit River | Project Engineer | Feasibility study for transient marina, conceptual dock layouts; final dock layout for permit application | 400 feet | \$17.7 Million |
| Townline Park Boat Launch and Beach | Lake Erie | Civil Engineer | Design and permitting of shoreline improvements, including detached breakwaters, boat ramps, parking lot and floating docks. | 750 feet | \$4.5 Million |
| KEENAN GIBBONS | | | | | |
| Mohawk River Pedestrian Bridge | Mohawk River (Amsterdam, NY) | Site Designer | A 500' x 30' curvilinear pedestrian bridge crosses the Mohawk River and connects to the Erie Canal. The bridge is characterized by a stamed 15-foot-wide path which meanders its way between raised seat walls harboring intensive vegetated planters (eg. Trees, shrubs, grasses), lighting, benches, and other amenities to create a park-like setting over the water. The bridge superstructure is steel and has formal gateways on either end that include an amphitheater and parking for bicycles and vehicles. Interpretive cultural signage and artwork celebrating the area's heritage are integrated throughout. | 500 feet x 30 feet | \$16.5 Million |
| DAN WAGNER, PE | | | | | |
| Peshawbestown Marina, New Construction | Grand Traverse Bay | Project Manager | The project consists of a 106 slip marina, with two fixed piers, four floating dock systems and broadside docking for a large vessel such as a Great Lakes cruise ship. One fixed pier measures approximately 925' in length and the other (which has been designed to function as a car ferry terminal in the future) measures approximately 1,050' in length. | 925 feet 1,050 feet | \$318,000 |
| Munson Medical Center Tributary A Relocation | Kid's Creek, GT Bay Watershed | Project Manager | Civil project manager responsible for all site work related design items including topographic and boundary survey work, geotechnical evaluation, utility relocation, earth operations/site grading, site pavements, storm water and soil erosion controls required to relocate approximately 3,000 lineal feet of an existing stream. The stream was relocated and restored to a natural cross section and alignment to complement future construction of a cancer treatment center. The project, which comprises the majority of two city blocks, also required the vacation of a platted alley and acquisition of several surrounding properties. During the construction phase GFA services included survey staking, underground utility installation observation, soil density/bearing capacity testing, concrete testing, structural inspection and miscellaneous site pavements. | n/a | \$120,000 |
| Eberta Yacht Club | Lake Michigan | Project Manager | Project manager responsible for final engineering for mixed use yacht club development. The project features a multi purpose building containing lodging, restaurant and community spaces. The project features a marina consisting of floating dock systems, fixed fishing pier and several boat houses. All permits and approvals were secured using our design drawings including municipal water, sewer, storm water controls, and DFC permits. | n/a | \$120,000 |
| GARY WILSON, P.S | | | | | |
| Veterans Pier, Tuscarora Township | Indian River, Inland Waterway | Survey | Survey and staking in support of new pier construction | 800 feet | \$3,500 |
| NMC Maritime Academy Improvements | Grand Traverse Bay | Survey | GFA created the survey work for the first phase of construction and subsequent phases of improvements. | Main pier: 600 feet x 50 feet | \$15,000 |
| Frenchtown Detroit Beach Sewall Structure | Lake Erie | Survey | Topographic mapping for US Army Corps of Engineers | 3,400 feet | \$22,800 |

| Name of Pier | Body of Water | Role | Brief Description | Length of Structure | Project Cost |
|--|--|--------|--|-------------------------|--------------|
| Green Bay, WI Wetland Rehabilitation | Delta/Lake Michigan | Survey | Topographic and hydrographic survey for US Army Corps of Engineers; including construction staking for five miles of fishing pier | 5 miles | \$40,000 |
| Harbor - Port Austin, MI | Lake Huron | Survey | Structure rehabilitation survey for the US Army Corps of Engineers | 2,300 feet | \$24,000 |
| Neeshish Island Rock Cut, St. Mary's River | Lake Superior, Lake Huron | Survey | Vertical laser scan of channel for US Army Corps of Engineers. The purpose of this scan was to create a high definition/high resolution laser scan of the Rock Cut channel walls and nearby shoreline. The data will be used to create a 3D CADD model of the existing rock walls for structural failure assessment, material inventory, and monitoring purposes. Scan included all waterward faces, tops, and all features immediately adjacent (rubble mounds, stockpiled blocks, etc). Initiated from and referenced to existing geodetic control. The laser scan referenced established and known features along the structure to facilitate the merging of scan data within existing CADD models. | 900 feet x 300 feet | \$25,000 |
| Sea Lamprey Barrier, Manistique MI | Manistique River (Lake Michigan) | Survey | US Army Corps of Engineers work order: 15 valley cross-sections, two bridge cross-sections and two dam cross-sections (one upstream face and one downstream face of dam) at the Manistique Papers Inc dam and the M-94 bridge on the Manistique River. Cross-sections to be used for hydraulic analysis and project design for a proposed sea lamprey barrier. | n/a | \$39,550 |
| Cass River Fish Passage Survey, Frankenmuth MI | Cass River | Survey | The purpose of this survey project was to obtain valley cross section channel data of the Cass River and the Frankenmuth Dam, to most accurately represent existing conditions in a hydraulic analysis. The modeling will determine what impacts are created by a 100-year flood event. In addition to the valley cross sections, project included seven levee cross sections, a structural survey of concrete dam features, and real estate survey information for US Army Corps of Engineers. | Dam: 23 feet x 235 feet | \$21,375 |
| Elmwood Township Marina | Grand Traverse Bay | Survey | New retaining wall, marina bottomlands and dock #3 | 500 feet | \$8,000 |
| Clinch Park Marina, Traverse City | Grand Traverse Bay | Survey | Construction staking and soundings, harbor master building, anchorage and new breakwall | 450 feet | \$25,000 |
| Detroit River, Bates St. Pier Outfall | Lake Erie | Survey | Topographic and hydrographic survey for the US Army Corps of Engineers | 200 feet | \$15,000 |
| Soo Locks, St. Mary's Falls Canalsee Control | St. Mary's River (Lake Superior, Lake Huron) | Survey | Updated mapping for the US Army Corps of Engineers | n/a | \$60,000 |
| Grand Traverse Bay Harbor, Houghton MI | Lake Superior | Survey | As-built for US Army Corps of Engineers rehabilitation study | 1,438 feet | \$8,000 |
| Harbor - Two Rivers WI | Lake Michigan | Survey | Mapping and control for US Army Corps of Engineers | 2,700 feet | \$12,000 |
| Chicago Harbor Breakwall Mapping | Lake Michigan | Survey | Updated mapping for the US Army Corps of Engineers | 10,000 feet | \$10,000 |

ANN ARBOR

CHICAGO

DALLAS

DETROIT

LOS ANGELES

MADISON

PHOENIX

SAN FRANCISCO

SHANGHAI

WASHINGTON, DC

SMITHGROUP JJR



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION REGULAR MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: ^{SD}JERED OTTENWESS, CITY MANAGER

SUBJECT: COMMUNITY DEVELOPMENT SERVICES AGREEMENT
WITH DDA AND GRAND TRAVERSE COUNTY

The DDA formed a staffing committee in mid-2013 following the passing of DDA Executive Director Bryan Crough to develop a staffing strategy moving forward. In December 2013, the staffing committee recommended to the DDA Board that they appoint Rob Bacigalupi as Executive Director and supplement the economic development role that Bryan Crough filled by entering into an agreement with Grand Traverse County and the City to provide those services through the Grand Traverse County Planning and Development Department. This effort resulted in the attached agreement that the DDA recently approved at its June 20th meeting. The Grand Traverse County Ways and Means Committee reviewed the Agreement at its July 16, 2014 meeting, and the Committee is recommending approval by the Grand Traverse County Board of Commissioners. The GTCBOC approved the agreement at its July 30, 2014, meeting.

The three-year agreement's scope of services includes the following key tasks specifically relevant to the City:

- Work with City boards to develop an implementation plan for corridors, using economic development tools including Land Bank Authority, Next Michigan Development Corporation, Brownfield Redevelopment Authority;
- Develop an economic strategic plan;
- Identify an economic development organization; and
- Establish an executive team from taxing jurisdictions to focus on tax increment financing policy

I believe this cooperative effort to be in the best interest of all entities concerned, both in terms of cost effectiveness and consolidation of effort. I recommend approving the agreement with an annual cost to the City of \$10,000 to be paid out of the Economic Development Fund.

I recommend the following motion (5 affirmative votes required):

that the Mayor and City Clerk execute the Community Development Services Agreement with the Downtown Development Authority and the County of Grand Traverse, which agreement shall be for a three year period, with an annual contribution of \$10,000 to be paid out of the Economic Development Fund, such agreement subject to approval as to its substance by the City Manager and its form by the City Attorney.

K:\tcclerk\city commission\agreements\development services grand traverse county

e-copy: Russell Soyring, Planning Director
 Rob Bacigalupi, DDA Executive Director
 John Sych, Grand Traverse County Director of Planning and
 Development
 Jean Derenzy, Grand Traverse County Deputy Director of Planning
 and Development

COMMUNITY DEVELOPMENT SERVICES AGREEMENT

THIS AGREEMENT made the _____ day of _____, 2014, by and between **CITY OF TRAVERSE CITY**, a Michigan municipal corporation, of 400 Boardman Avenue, Traverse City, Michigan 49684, (the "City") the **DOWNTOWN DEVELOPMENT AUTHORITY**, a Michigan authority, 303 East State Street, Suite C, PO Box 42, Traverse City, Michigan, 49685 (the "DDA"), and the **COUNTY OF GRAND TRAVERSE**, a Michigan corporation, of 400 Boardman Avenue, Traverse City, Michigan (49684), (the "County");

WITNESSETH

WHEREAS, the City is authorized to plan for economic development within the City in the interest of the public health, safety and welfare; and

WHEREAS, the DDA is authorized under the Downtown Development Authority Act, MCL 125.1651 *et seq* (the "Act") to study and analyze the impacts of metropolitan growth on the downtown districts (the "Districts") and develop long-range plans designed to halt the deterioration of property values in the Districts and to promote the economic growth of the Districts; and

WHEREAS, the County, through its Department of Planning and Development, is authorized to plan for economic development within the County in the interest of the public health, safety and welfare; and

WHEREAS, the DDA, the City, and the County may make and enter into contracts necessary or incidental to accomplish this purpose; and

WHEREAS, the public and future downtown, City, and County development are served by the County providing the City and the DDA with community development services as set forth herein; and

WHEREAS, this Agreement is entered into pursuant to the Intergovernmental Contracts between Municipal Corporations Act, MCL 124.1 *et seq*.

NOW THEREFORE, the City, the DDA, and the County agree as follows:

1. Scope of Services. The County shall provide the services set forth in Attachment A, attached hereto and made a part hereof, through its Planning and Development Department to the City and the DDA (the "Services"), which services may from time to time be amended by the parties.

2. Term. The term of this Agreement shall be 3 years from the date of this Agreement. Thereafter, the term of this Agreement may be extended by mutual agreement for an additional period as may be agreed by the parties.

3. Compensation. Each party to this Agreement shall contribute an amount of ten thousand dollars (\$10,000.00) annually during the term of this Agreement. For any additional term, compensation shall be in an amount as determined by the parties by mutual agreement.

4. No Transfer. No transfer of personnel, ownership of personal or real property, leases, or contracts is intended by this Agreement. The relationship of the parties is that of an independent contractor and in accordance therewith, the parties covenant and agree to conduct themselves consistent with such status and that neither the parties nor their employees, officers or agents will claim to be an officer, employee or agent of the other or make any claim, demand or application to or for any rights or privileges applicable to any officer or employee of same, including but not limited to worker's compensation coverage, unemployment insurance benefits, social security coverage, or retirement membership or credit. The parties do not intend the services provided by the County to be a joint venture.

5. Non Discrimination. The parties agree not to discriminate against an employee or applicant for employment with respect to hire, tenure, terms, conditions or privileges of employment, or a matter directly or indirectly related to employment because of race, color, religion, national origin, age, sex, height, weight, sexual orientation, handicapped status, or marital status. Breach of this covenant may be regarded as a material breach of this Agreement. The parties additionally agree not to discriminate on the above basis in their administration of this Agreement.

6. Termination. Any party may terminate this Agreement in whole or in part whenever the parties determine that termination is in their best interest upon 30 days written notice to the other parties. Upon termination, the County shall be entitled to and the City and the DDA shall pay the costs actually incurred in compliance with this Agreement up to the date of termination.

7. Failure to Enforce. Failure by the parties at any time to enforce any provision of this Agreement shall not be construed as a waiver of any such provisions. Such failure to enforce shall not affect the validity of the Agreement or any part thereof, or the right of the parties to enforce any provision at any time in accordance with its terms.

8. Dispute Resolution. If any party has a dispute with another regarding the meaning, operation or enforcement of this Agreement any provision of this Agreement, the disputing parties agree to meet and confer to negotiate a resolution of the dispute. They further agree if they are unable to resolve the dispute themselves and before formally instituting any other dispute mechanism, they shall utilize the services of a mutually acceptable neutral mediator, who meets the qualifications of MCR 2.411, to bring them together in at least one mediation session.

9. Third Party Beneficiaries. This Agreement confers no rights or remedies on any third party, other than the parties to this Agreement and their respective successors and permitted assigns.

10. Digital Signatures. The parties hereto acknowledge and agree under the Uniform Electronic Transactions Act, MCL 450.832, et seq. that this Agreement may be executed with the electronic signature of any person authorized and required to sign on behalf of the parties hereto.

11. Execution in Counterparts. This Agreement may be executed in counterparts, each of which shall be an original and all of which shall constitute the same instrument.

12. Entire Agreement. This Agreement, together with all the items incorporated herein by reference constitutes the entire agreement of the parties and there are no valid promises, conditions or understandings that are not contained herein.

CITY OF TRAVERSE CITY

Michael Estes, Mayor

Benjamin C. Marentette, City Clerk

DOWNTOWN DEVELOPMENT AUTHORITY

, Chairperson

, Secretary

GRAND TRAVERSE COUNTY

, Chair

, Clerk

Approved as to Substance:

Jered Ottenwess, City Manager

Rob Bacigalupi, DDA Executive Director

Dave Benda, County Administrator

Approved as to Form:

Lauren Tribble-Laucht, City Attorney/General Counsel

Robert Cooney, General Civil Counsel

COMMUNITY DEVELOPMENT

SCOPE OF SERVICES

Concepts and Approaches

Pursue economic development initiatives to spur job growth, increase income for local residents, raise property values, and expand the tax base.

These services as outlined may be revised or amended from time to time as agreed upon by the parties.

Approaches to Public-Sector Economic Development

Implementation Tasks

Developments within DDA District: Work with private developers on existing and new plan concepts / new investment within the DDA and management of these projects. These services will include working with developers to review development plans and recommend public economic tools needed, including Brownfield, Land Bank, Next Michigan, etc. Work will be coordinated with DDA Director to achieve success to implement capital plan/master plan goals for public infrastructure. Coordinate activities and develop Plan that identifies joint ventures between DDA, State and other entities.

City Wide Development: Work with City Planning Commission, City Commission and County Board to develop an implementation plan for the established transportation corridors. It is the intent that the initial corridors where work will be focused are identified in Exhibit 1, attached hereto and made a part hereof, (the "Corridors"). The Corridors may be modified from time to time depending on need, opportunity, and benefit as determine by the parties.

Economic tools will include, but not be limited to, Land Bank (Land assembling strategic property purchases), Brownfield, and Next Michigan (utilizing the Local Development Financing Authority {LDFA} tool through Next Michigan). These corridors are recognized as neighborhood mixed-use districts that provide residents with goods and services that tie into downtown.

Develop Economic Strategic Plan: This Strategic Plan will identify the values of the community, realize the limits of government and establish joint ventures to implement economic opportunities; work to support long term jobs for Grand Traverse County residents and reinforce the County's key role in the regional economy; reinforce the message that businesses are a vital part of the community; support growth of local businesses, both small and large, in technology and non-tech fields; leverage the assets of higher education to build economic opportunities; and create a County for the next generation. Support investment in our community that creates new jobs; enhance commercial districts that provide residents with goods and services for the neighborhood(s) (ties directly back to the Implementation of Corridors); and cultivate an entrepreneurial and academic environment that fosters innovation and the encouragement to grow the cultural assets and entertainment activities that attract visitors and conventions.

Draft 6/13/14

The above outlined activities are identifiably broad with components that can, and should be, drawn out as strategies to implement the broad goal(s) of each category. It is also recommended that on a yearly basis that a report be provided to be able to see how the activities have obtained results. It is by this measurement that boards, committees, councils and staff can implement new (change) strategies to help with the implementation of goals.

Economic Development Organization (EDO) A result of the Economic Development Strategy may be the identification of an EDO to market and promote the County and region. This may include examining a variety of possible models; identifying additional community partners; making a recommendation and outlining the process for achieving this goal, if adopted.

Establish Executive Team from all taxing jurisdictions: Team will focus on TIF Policy, and assist in the coordination of "opting in" with new LDFA and/or any TIF program expansion. Team will be comprised of representatives from other tax-supported governmental units within the region. This team is critical to achieve successful implementation of any future TIF being LDFA or DDA and support from the local taxing jurisdictions, having leaders from each of the organizations is important to ensure communication is sent back to respective boards.

Management Tasks

Maintain Communications Between Staff Participation in meetings with DDA Director, City Manager, County Administrator.

Provide Regular Updates to Boards and Commissions

Measuring Results of Project Progress

It is important that plans/projects have tangible outputs that can be benchmarked and measured. Useful data includes labor statistics, retail sales, tax revenue changes, commercial occupancy rates, property and rent values, and the total number of new business starts. In addition surveys to track consumer and investor opinions should be utilized to provide feedback to policy makers. County will work with DDA and City to benchmark and measure success.

The City of Traverse City

Code Enforcement

Governmental Center
400 Boardman Ave
Traverse City MI 49684
(231) 922-4414
www.traversecitymi.gov



QUARTERLY REPORT FOR MAY-JULY 2014
MIKE TROMBLEY CEO

| COMPLAINTS for 2014 | MAY (14-31) | JUNE | JULY(1-17) | |
|-------------------------------------|-------------|------------|-------------|--|
| Lawns | 3 | 20 | 5 | |
| Sign Violations | 2 | 1 | 0 | |
| Sidewalk Café Inspections | 2 | 6 | 5 | |
| Brush/ Streets Dpt. | 2 | 17 | 4 | |
| Parking violations | 3 | 13 | 8 | |
| Neighbor Disputes | 5 | 8 | 3 | |
| Street Performer inspections/compl. | 1 | 4 | 1 | |
| Commissioner/Mayor Complaints | 1 | 4 | 4 | |
| Chickens | 1 | 1 | 0 | |
| Trash/garbage/junk | 4 | 9 | 4 | |
| Smoking Violation | 2 | 0 | 0 | |
| Dilapidated Homes/buildings | 1 | 5 | 0 | |
| Food Trucks/Vendors | 3 | 0 | 0 | |
| Noise | 1 | 0 | 0 | |
| Health/Safety | 1 | 6 | 0 | |
| News Racks | 0 | 2 | 0 | |
| DDA issues/complaints | 0 | 4 | 8 | |
| | | | | |
| TOTALS | 32 | 100 | 41 | |

The above chart shows the type and amount of each complaint that was handled. No citations were issued, only warnings, as almost all violations were corrected after one warning. I have not begun to do any Rental Housing Inspections, however will be sending out letters this week to schedule and begin the weekly inspections. Approximately 90% of the above complaints were call generated, and 10% self initiated.

**TRAVERSE CITY
LIGHT AND POWER BOARD**

Minutes of Regular Meeting
Held at 5:15 p.m., Commission Chambers, Governmental Center
Tuesday, June 24, 2014

Board Members -

Present: Barbara Budros, Jim Carruthers, Jan Geht, Jeff Palisin, Bob Spence, John Taylor

Absent: Patrick McGuire

Ex Officio Member -

Present: Jered Ottenwess, City Manager

Others: Tim Arends, Scott Menhart, Karla Myers-Beman, Tom Olney, Kelli Schroeder, Stephanie Tvardek, Jessica Wheaton, Blake Wilson

The meeting was called to order at 5:15 p.m. by Chairman Taylor.

Jim Carruthers requested item 2b be removed from the Consent Calendar for full discussion.

Item 2 on the Agenda being Consent Calendar

Moved by Carruthers, seconded by Palisin, that the following actions, as recommended on the Consent Calendar portion of the Agenda, as amended, be approved:

- a. Minutes of the Regular Meeting of June 11, 2014.
- b. *Removed from the Consent Calendar.*
- c. Third Amendment to the Munson Dark Fiber Services Agreement.

CARRIED unanimously. (McGuire absent)

Items Removed from the Consent Calendar

- a. Consideration of renewal of the AT&T Phone Contract.

The following individuals addressed the Board:

Scott Menhart, Manager of Telecom and Technology

Moved by Geht, seconded by Budros, that the Board authorizes the Executive Director to execute a three year contract with AT&T in the amount of \$19,080.

CARRIED unanimously. (McGuire absent)

Item 3 on the Agenda being Old Business

- a. Public Hearing regarding proposed rate increase July 1, 2014.

The following individuals addressed the Board:

Karla Myers-Beman, Controller
Tim Arends, Executive Director

The Chairman opened the Public Hearing:

The following individuals from the public addressed the Board:

Michael Estes, Mayor of Traverse City, Ratepayer, Expressed Support
Rick Buckhalter, 932 Kelley Street, Ratepayer – Expressed Support

The Chairman closed the Public Hearing

Board discussion followed.

Moved by Budros, seconded by Palisin, that the Board authorizes an overall 1.5% electric service rate increase with a bandwidth increase and decrease of no more than 1.3% as proposed by Utility Financial Solutions along with the housekeeping changes to be implemented, as presented, effective July 1, 2014.

CARRIED unanimously. (McGuire absent)

Item 4 on the Agenda being New Business

- a. Consideration of After-Hours Dispatch Agreement with Cherryland Electric Cooperative.

The following individuals addressed the Board:

Tim Arends, Executive Director
Frank Siepker, Cherryland Electric, Engineering and Operations Manager

Moved by Budros, seconded by Spence, that the Board authorizes the Chairman and Secretary to enter into a contract with Cherryland Electric Cooperative that would allow TCL&P to provide after-hours crew dispatch services to Cherryland Electric Cooperative; subject to approval as to substance by the Executive Director and as to form by General Counsel.

CARRIED unanimously. (McGuire absent)

Item 5 on the Agenda being Reports and Communications

- A. From Legal Counsel.

None.

- B. From Staff.

1. Tim Arends and Michael McGeehan, GRP Engineering, Inc. reviewed the need for the West Side Transmission Line.

Chairman Taylor asked for the Board's approval to suspend the board rules and allow public comment after item 5B1.

Moved by Carruthers, seconded by Spence, that the board rules be suspended to allow public comment after item 5B1.

CARRIED unanimously. (McGuire absent)

Item 6 on the Agenda being Public Comment

Emily Mitchell, 3716 Jefferson, Hickory Meadows Advisory Committee, Non-Ratepayer
Rick Buckhalter, 932 Kelley Street, Ratepayer
Nicole Gerard, 2973 Concord Street, Hickory Meadows Advisory Committee, Non-Ratepayer
Barbara Solmonson, 119 North Madison, Ratepayer
Mike Sleeman, 3680 Incochee Rd.
Cynthia Burzak, Elmwood/Wayne, Ratepayer
Mike Gaines, 425 North Madison, Ratepayer
Jamie Al-Shamma, 131 North Elmwood, Ratepayer
Pat Joseph, 502 North Spruce

General discussion amongst Board and staff continued regarding item 5B1.

Item 5 on the Agenda being Reports and Communications (continued)

6:55 p.m. Chairman Taylor called the meeting to recess.

7:01 p.m. Chairman Taylor called the meeting to order.

2. Energy Supply summary presentation by RTD Consulting and Howard Axelrod.

The following individuals addressed the Board:

Tim Arends, Executive Director

C. From Board.

1. Chairman Taylor proposed the cancelation of the July 8, 2014 regular meeting. It was the consensus of the Board to cancel.

There being no objection, Chairman Taylor declared the meeting adjourned at 7:43 p.m.



Tim Arends, Secretary
LIGHT AND POWER BOARD

/st

August 4- cc Refs



MONTHLY OPERATING REPORT

TO: Jered Ottenwess, Traverse City Manager
Mike Slater, Grand Traverse County

COPY: Grand Traverse County Board of Public Works
John Bowman, CH2M HILL, Regional Business Manager
Dave Green, Director of Public Services

FROM: Casey Rose, CH2M HILL Project Manager *CR*

DATE: July 11, 2014

SUBJECT: Monthly Operations Report for June 2014

Attached is a copy of the report to the state, which we submit electronically.

The data sheet showing flows and loadings from the Townships, the City, and the Septage Plant for June is attached to this report. Also attached is financial information as of the last Friday in June.

TRAVERSE CITY

OPERATIONS AND PROJECTS

The treatment plant experienced an Unauthorized Discharge on June 5, 2014. Chlorine used for cleaning mixed with plant effluent causing a chlorine residual in the effluent. The discharge was short term and there were no environmental impacts. MDEQ was notified, they requested CH2M Hill report this as an Unauthorized Discharge because our NPDES permit doesn't set a chlorine residual limit.

We continue our annual recovery cleans for each membrane train.

Annual Inspections of the first four membrane trains is nearing completion. There hasn't been any major maintenance required.

We continue to relocate the cassettes in Train #1 in preparation of installing the new membranes.

Maintenance

We replaced the breaker for Screw Pump #3. While the pump was down we replaced the thrust bearing on the motor. The output bearing will need replacement later this summer.

The motor for Primary Tank #4 north was repaired and placed back into service.

IPP

We filed suit against Ambrosia Café and Brewery Ferment last month for failing to install the required inspection manhole. Ken Stawowy is listed as the complaining witness for this hearing, which is set to take place on July 14th.

We visited Right Brain Brewery during the month and observed the water from their spent grain leaking out of large garbage cans on their back loading dock. This water was draining down into the street, which could eventually be washed into the storm sewer and then Boardman Lake. We spoke with Russell Springstead, Right Brain's owner, about the issue and he agreed to stop the dripping.

We received a complaint for the alley behind the Union Street Cantina. It appeared that the grease vender spilled a large amount of grease on the ground when they dumped the bulk grease dumpster. There was some attempt to clean the mess up but it was still pretty slippery. Justin Roy was notified, as it was a possible threat to the storm sewer, but the sanitary sewer didn't seem to be in any danger.

Simon of Roaming Harvest, on State Street, was reminded that he needed to install an inspection manhole. He stated that he would get one installed, but has yet provided a copy of a quote.

Jena Hullman was called again to remind her of her obligation to install an inspection manhole at Elixir Café. She hasn't returned the call.

A discharge permit was issued to Shorts Brewing in Elk Rapids to bring the waste water from their brewing process to the Septage Facility while they are completing the construction of their pretreatment facility.

The groundwater discharge from Hotel Indigo has stopped. According to their final discharge report, they discharged a total of 96,400,976 gallons of groundwater to the collection system since June 13th, 2013. During that time, they demonstrated 70 violations of their discharge permit for high cyanide levels and 14 violations for higher than permitted flow rates. There were also several administrative violations, such as 61 violations for failing to cease discharge once a permit violation was detected and 9 violations for discharging without a permit.

Clark Manufacturing was inspected in June as a possible source for the continued oily discharge in Coast Guard Lift Station. The main floor drain was full of oil and needed to be cleaned. Follow up has yet to be scheduled.

OTHER

With Hotel Indigo no longer discharging flow decreased 18 million gallons compared to last month, but up 8 million gallons from last year.

GRAND TRAVERSE SEPTAGE FACILITY

Operations

Total flow from the Septage Facility to the Regional Plant was 869,300 gallons with an average of 9.36 lbs/day BOD in June.

Windemuller has been trouble shooting the new card reader and should have in service in July.

Maintenance

The influent pump was plugged. The pump was removed and cleaned.

Other

Lawn cutting services have been coordinated and the grass is now being mowed.

Michigan Department of Environmental Quality Monthly Discharge Monitoring Report (DMR)

PERMITTEE NAME: Traverse City WWTP
 MAILING ADDRESS: 606 Franklin Street
 Traverse City, MI 49684
 FACILITY: Traverse City WWTP
 LOCATION: 606 Hannah
 Traverse City, MI 49686

PERMIT NUMBER: M10027481
 MONITORING POINT: 001A

DISTRICT: Cadillac
 COUNTY: Grand Traverse

Monitoring Period: 2014-06-01 To: 2014-06-30
 NO DISCHARGE FROM SITE: ()

| Parameter | Quantity or Loading | | Units | Quality or Concentration | | Units | No. Ex. | Frequency of Analysis | Sample Type |
|---|-------------------------------------|-------------------------------|---------|--------------------------|-------------------------------------|-------|---------|-----------------------|-------------------------|
| | Average | Maximum | | Minimum | Maximum | | | | |
| Flow | 4.629 | 4.962 | | ***** | ***** | | 0 | Daily | Report Total Daily Flow |
| PARAM CODE: 50050 Mon. Site No.: 001A Stage Code: 1 | (report) Maximum Monthly Average | (report) Maximum Daily | MGD | ***** | ***** | ***** | | Daily | Report Total Daily Flow |
| Total Suspended Solids | 0.0 | 140 | | ***** | 3.7 | | 0 | 3X Weekly | 24-Hr Composite |
| PARAM CODE: 00530 Mon. Site No.: 001A Stage Code: 1 | 2100 Maximum Monthly Average | 3200 Maximum 7-Day Average | lbs/day | ***** | 45 Maximum 7-Day Average | mg/l | | 3X Weekly | 24-Hr Composite |
| Carbonaceous Biochemical Oxygen Demand (CBOD5) | 0.0 | 97 | | ***** | 2.4 | | 0 | 3X Weekly | 24-Hr Composite |
| PARAM CODE: 80082 Mon. Site No.: 001A Stage Code: 1 | 1800 Maximum Monthly Average | 2800 Maximum 7-Day Average | lbs/day | ***** | 40 Maximum 7-Day Average | mg/l | | 3X Weekly | 24-Hr Composite |
| Ammonia Nitrogen (as N) | 0.0 | ***** | | ***** | 0.0 | | 0 | 3X Weekly | 24-Hr Composite |
| PARAM CODE: 00610 Mon. Site No.: 001A Stage Code: 1 | 780 Maximum Monthly Average | ***** | lbs/day | ***** | 11 Maximum Monthly Average | mg/l | | 3X Weekly | 24-Hr Composite |
| Total Phosphorus (as P) | 15 | ***** | | ***** | 0.39 | | 0 | 5X Weekly | 24-Hr Composite |
| PARAM CODE: 00665 Mon. Site No.: 001A Stage Code: 1 | 36 Maximum Monthly Average | ***** | lbs/day | ***** | 0.5 Maximum Monthly Average | mg/l | | 3X Weekly | 24-Hr Composite |
| Total Copper | ***** | ***** | | ***** | *G | | 0 | Quarterly | 24-Hr Composite |
| PARAM CODE: 01042 Mon. Site No.: 001A Stage Code: 1 | ***** | ***** | ***** | ***** | (report) Maximum Monthly Average | ug/l | | Quarterly | 24-Hr Composite |
| Total Mercury | ***** | ***** | | ***** | (report) Maximum Monthly Average | ng/l | 0 | Quarterly | 24-Hr Composite |
| PARAM CODE: 71900 Mon. Site No.: 001A Stage Code: 1 | ***** | ***** | ***** | ***** | Maximum Monthly Average | ng/l | | Quarterly | 24-Hr Composite |

Name/Title of Principal Executive Officer Or Authorized Agent
 Casey Rose/Project Manager
Signature of Principal Executive Officer Or Authorized Agent
 Elizabeth Hart
Telephone No
 231-922-4922
Date (MM/DD/YYYY)
 2014-07-18

When completed mail this report to: PCS-Data Entry, MDEQ-WD, P.O. Box 30273, Lansing, MI, 48909-7773

Michigan Department of Environmental Quality Discharge Monitoring Report (DMR)

PERMITTEE NAME: Traverse City WWTP
 PERMIT NUMBER: M10027481
 DISTRICT: Cadillac

Grand Traverse

COUNTY:

001A

MONITORING GROUP:

NO DISCHARGE FROM SITE: ()

Monitoring Period: 2014-06-01 To: 2014-06-30

MAILING ADDRESS: 606 Franklin Street
 Traverse City, MI 49684
 FACILITY: Traverse City WWTP
 LOCATION: 606 Hannah
 Traverse City, MI 49686

| Parameter | Quantity or Loading | | Quality or Concentration | | Units | No. Ex. | Frequency of Analysis | Sample Type |
|---|---------------------|---------|--------------------------|--------------------------------------|------------------------------------|---------|-----------------------|-------------|
| | Average | Maximum | Minimum | Average | | | | |
| Fecal Coliform | ***** | ***** | ***** | 13.8 | 23.4 | 0 | 3X Weekly | Grab |
| PARAM CODE: 74055 Mon. Site No.: 001A Stage Code: 1 | ***** | ***** | ***** | 200 Max Monthly Geometric Mean | 400 Max 7-Day Geometric Mean | | 3X Weekly | Grab |
| pH | ***** | ***** | 6.8 | ***** | 7.0 | 0 | 3X Weekly | Grab |
| PARAM CODE: 00400 Mon. Site No.: 001A Stage Code: 1 | ***** | ***** | 6.0 | ***** | 9.0 | | 3X Weekly | Grab |
| Dissolved Oxygen | ***** | ***** | 8.7 | ***** | ***** | 0 | 3X Weekly | Grab |
| PARAM CODE: 00300 Mon. Site No.: 001A Stage Code: 1 | ***** | ***** | 4.0 | ***** | ***** | | 3X Weekly | Grab |
| CBOD5 Minimum % Removal | ***** | ***** | 98.9 | ***** | ***** | 0 | Monthly | Calculation |
| PARAM CODE: 80091 Mon. Site No.: 001A Stage Code: K | ***** | ***** | 85 | ***** | ***** | | Monthly | Calculation |
| Total Suspended Solids Minimum % Removal | ***** | ***** | 98.6 | ***** | ***** | 0 | Monthly | Calculation |
| PARAM CODE: 81011 Mon. Site No.: 001A Stage Code: K | ***** | ***** | 85 | ***** | ***** | | Monthly | Calculation |

Name/Title of Principal Executive Officer Or Authorized Agent: Casey Rose/Project Manager
 Signature of Principal Executive Officer Or Authorized Agent: Elizabeth Hart
 Telephone No: 231-922-4922
 Date (MM/DD/YY): 2014-07-18

When completed mail this report to: PCS-Data Entry, MDEQ-WD, P.O. Box 30273, Lansing, MI, 48909-7773

Page 2

TOWNSHIP AND CITY BOD CONCENTRATIONS LOADINGS AND FLOWS 2013-2014

| BOD average Concentrations | Average | | | | | | | | | | | |
|-------------------------------|---------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|----------------|
| | Jul-13 | Aug-13 | Sep-13 | Oct-13 | Nov-13 | Dec-13 | Jan-14 | Feb-14 | Mar-14 | Apr-14 | May-14 | Jun-14 to date |
| Blair Twp | 249 | 268 | 228 | 237 | 231 | 255 | 270 | 273 | 265 | 226 | 222 | 254 |
| Garfield Twp | 381 | 543 | 253 | 230 | 209 | 238 | 273 | 247 | 184 | 165 | 179 | 203 |
| Elmwood Twp | 277 | 323 | 433 | 437 | 237 | 276 | 237 | 258 | 229 | 221 | 247 | 251 |
| East Bay Twp | 251 | 402 | 211 | 189 | 162 | 164 | 191 | 207 | 144 | 119 | 133 | 144 |
| Acme Twp | 151 | 146 | 140 | 139 | 158 | 161 | 167 | 161 | 111 | 135 | 120 | 125 |
| Peninsula twp | 289 | 201 | 207 | 196 | 205 | 215 | 217 | 223 | 188 | 196 | 225 | 243 |
| City | 469 | 184 | 36 | 81 | 1114 | 214 | 1104 | 121 | 76 | 28 | 24 | 38 |
| Septage | | | | | | | | | | | | 314 |

* We have no BOD data for Blair Township. Loadings are calculated using plant influent BOD as a surrogate for Blair Twp BOD concentration

| BOD Loadings in pounds per day | Average | | | | | | | | | | | |
|-----------------------------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|----------------|
| | Jul-13 | Aug-13 | Sep-13 | Oct-13 | Nov-13 | Dec-13 | Jan-14 | Feb-14 | Mar-14 | Apr-14 | May-14 | Jun-14 to date |
| Blair | 404 | 404 | 404 | 42 | 36 | 35 | 39 | 38 | 37 | 35 | 41 | 48 |
| Garfield Twp | 4870 | 4465 | 2533 | 2604 | 2437 | 2623 | 3151 | 2858 | 2942 | 2598 | 2507 | 2784 |
| Elmwood Twp | 895 | 780 | 306 | 290 | 322 | 286 | 348 | 270 | 221 | 295 | 334 | 320 |
| East Bay Twp | 1562 | 1523 | 1309 | 809 | 657 | 446 | 418 | 414 | 389 | 408 | 597 | 485 |
| Acme Twp | 1761 | 1808 | 481 | 397 | 298 | 293 | 392 | 380 | 288 | 257 | 252 | 342 |
| Peninsula Twp | 406 | 406 | 61 | 60 | 68 | 71 | 81 | 65 | 51 | 83 | 71 | 61 |
| Septage | 202 | 202 | 7 | 19 | 199 | 21 | 74 | 8 | 6 | 4 | 7 | 9 |
| City | 10100 | 10504 | 4945 | 4388 | 4561 | 4710 | 4013 | 4442 | 4027 | 4682 | 5398 | 5178 |
| Total | 20200 | 10217 | 9256 | 8610 | 8579 | 8486 | 8517 | 8475 | 7962 | 8361 | 9207 | 9228 |
| Blair | 0.170 | 0.170 | 0.024 | 0.023 | 0.020 | 0.018 | 0.020 | 0.019 | 0.021 | 0.021 | 0.023 | 0.024 |
| Garfield Twp | 2.050 | 1.879 | 1.285 | 1.265 | 1.222 | 1.204 | 1.374 | 1.230 | 1.299 | 1.343 | 1.299 | 1.259 |
| Elmwood Twp | 0.376 | 0.374 | 0.145 | 0.151 | 0.185 | 0.144 | 0.153 | 0.131 | 0.144 | 0.214 | 0.224 | 0.189 |
| East Bay Twp | 0.657 | 0.641 | 0.243 | 0.222 | 0.333 | 0.194 | 0.212 | 0.193 | 0.204 | 0.221 | 0.290 | 0.232 |
| Acme Twp | 0.741 | 0.761 | 0.273 | 0.252 | 0.220 | 0.214 | 0.247 | 0.220 | 0.240 | 0.259 | 0.227 | 0.285 |
| Peninsula Twp | 0.171 | 0.171 | 0.052 | 0.052 | 0.052 | 0.053 | 0.058 | 0.048 | 0.055 | 0.074 | 0.071 | 0.059 |
| Septage | 0.085 | 0.085 | 0.023 | 0.028 | 0.021 | 0.012 | 0.008 | 0.008 | 0.009 | 0.015 | 0.034 | 0.030 |
| City | 4.250 | 4.420 | 2.866 | 2.678 | 2.665 | 2.623 | 2.220 | 2.386 | 2.573 | 2.865 | 2.873 | 2.552 |
| Total | 8.500 | 5.021 | 4.911 | 4.671 | 4.718 | 4.463 | 4.291 | 4.234 | 4.546 | 5.013 | 5.041 | 4.629 |

Average Daily
Flows, MGD

Traverse City Regional Wastewater Treatment Plant 2013-2014

| | June | | | Year-to-date | |
|---------------------------------|----------------|----------------|---------------|------------------|------------------|
| | Budget | Actual | Variance | Total Budget | Total Actual |
| CHEMICALS | 15,583 | 4,457 | 11,126 | 186,990 | 182,317 |
| CLIENT PLANT AND EQUIPMENT | 9,583 | 2,031 | 7,553 | 115,000 | 112,357 |
| DUES AND SUBSCRIPTIONS | 40 | 58 | -18 | 482 | 509 |
| EDUCATION, TRAINING, MEETINGS | 465 | 331 | 134 | 5,581 | 8,126 |
| ELECTRICITY | 33,170 | 43,195 | -10,025 | 398,040 | 455,409 |
| EMPLOYEE EXPENSE | 602 | 1,319 | -717 | 7,220 | 14,464 |
| INSURANCE | 2,534 | 1,960 | 573 | 30,404 | 31,557 |
| LABOR | 69,159 | 52,963 | 16,196 | 841,435 | 830,817 |
| OPERATING EXPENSES | 9,895 | 11,596 | -1,701 | 119,237 | 208,593 |
| OUTSIDE SERVICES | 2,458 | 7,228 | -4,770 | 29,500 | 76,443 |
| SOLIDS HANDLING | 460 | 10,642 | -10,182 | 150,420 | 153,610 |
| SUPPLIES | 1,417 | 719 | 698 | 17,000 | 8,246 |
| TELECOMMUNICATIONS | 739 | 222 | 517 | 8,870 | 7,697 |
| TRAVEL COSTS | 1,725 | 0 | 1,725 | 20,702 | 35,260 |
| UTILITIES-OPERATIONS | 1,713 | 1,218 | 494 | 20,550 | 18,574 |
| Total Cost | 149,542 | 137,940 | 11,602 | 1,951,430 | 2,143,978 |
| | | | | | |
| REPAIRS (included in the above) | 9,583 | 2,298 | 7,285 | 115,000 | 137,896 |
| | | | | | -22,896 |



The City of Traverse City

Communication to the City Commission

FOR THE CITY COMMISSION MEETING OF AUGUST 4, 2014

DATE: JULY 31, 2014

FROM: JO JERED OTTENWESS, CITY MANAGER

SUBJECT: PUBLIC COMMENT REQUEST – LAVERN BROUGHTON –
OATH OF OFFICE GIVEN BY CITY CLERK

Attached is a reserved public comment request from LaVern Broughton, 611 Hastings Street, regarding “Oath of Office Given by the City Clerk... violations of City Charter, by the City Commissioners...”

As provided in the City Commission Rules, fifteen (15) minutes have been allocated.

JJO/bcm

e-copy: LaVern Broughton

k:\tcclerk\city commission\public comment\reserved_broughton

7/27/2014

To: Mr. Jered Ottenwess
Traverse City Manager.

From: LaVern P. Broughton

Dear Mr. Jered Ottenwess Would you please put me on the City Agenda on The next regular City Commission meeting. subjects to be the Oath of Office given by the City Clerk . Violations of City Charter, By the city Commissioners. If it is possible I would like to be on the agenda. After 8:30 Pm. My wife Helen has Parkinsons and I don't leave the Grand Traverse Pavilion until after 7:30 pm. Thank You.

LaVern P. Broughton

LaVern P. Broughton
611 Hastings St.
Traverse City, mi
Ph.(231) 947-8713

CITY MANAGER'S OFFICE

JUL 21 2014

RECEIVED